

west capitol avenue

DESIGN GUIDELINES



2007

West Capitol Avenue and Central Business District Design Guidelines

2007 STREETScape MASTER PLAN

September 2007

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acknowledgements

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chapter 1

west capitol avenue

DESIGN GUIDELINES



design guidelines

IN THIS CHAPTER

PRIVATE REALM

BUILDING CHARACTER

PARKING

SERVICE AREAS AND ACCESS

PUBLIC REALM

STREETS

SIDEWALKS

CURB CUTS

CROSSWALKS

MEDIANS

LANDSCAPING OF SIDEWALKS AND
MEDIANS

STREET LIGHTING

STREET FURNITURE

SIGNAGE

BICYCLE PARKING

PUBLIC TRANSIT

These design guidelines are a tool to ensure that built and open spaces are conceived and constructed in accordance with the urban design concept described in the previous volume. The guidelines will be used to inform design processes and produce the highest caliber development. They will also create compatibility in the environment, both public and private, through sensitive architecture and site design.

The guidelines are meant to be flexible, yet effective means of revitalizing the corridor. A range of implementation options is provided, and a concerted effort has been made to avoid prescriptive guidelines that would stifle creativity.

Specific Site Design and Planning Guidelines have been divided into the following categories:

1. Site Design and Planning of the Private Realm
2. Site Design and Planning of the Public Realm
3. Infrastructure (presented in Chapter 2)

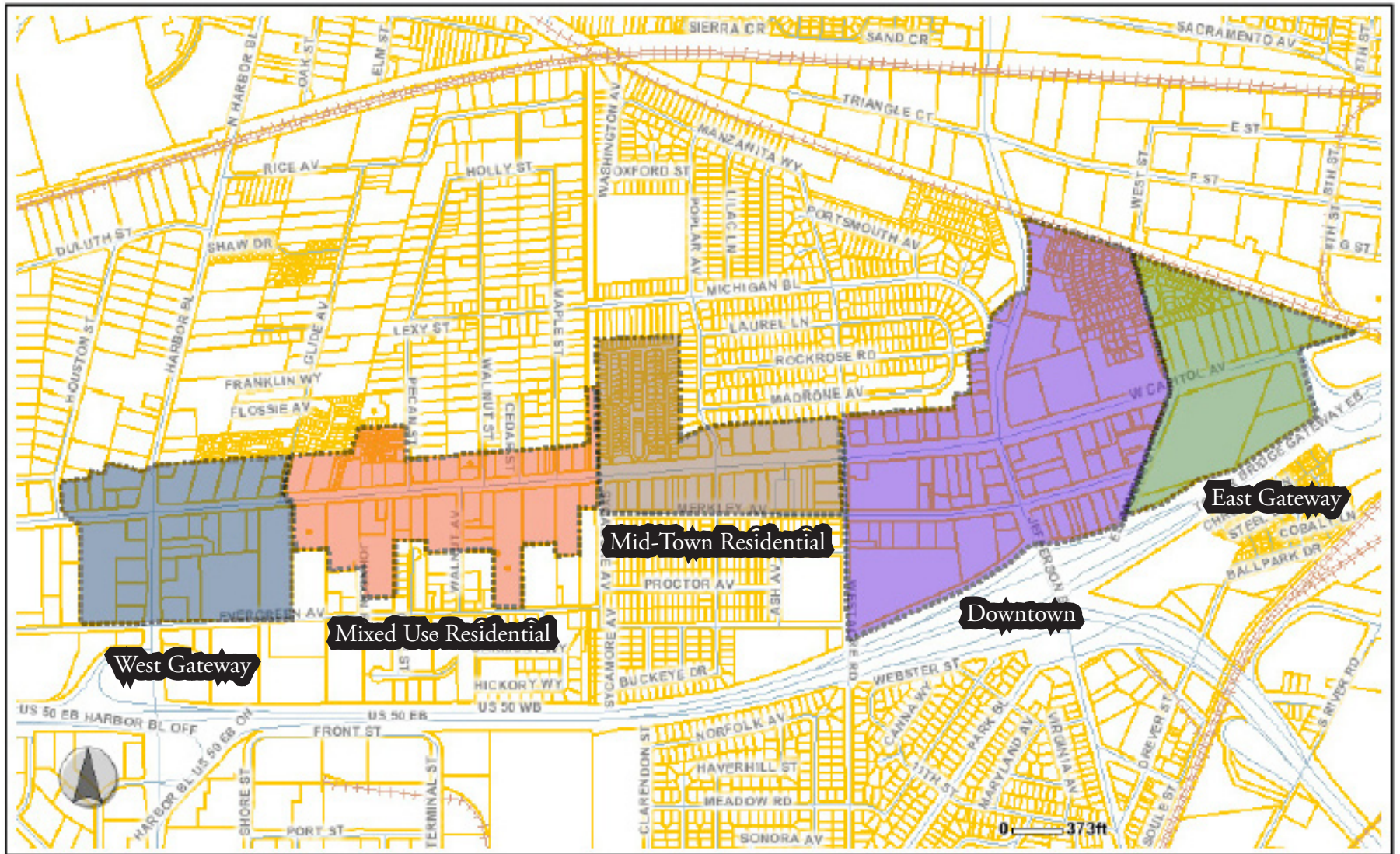


Applicability

WEST CAPITOL AVENUE AND CENTRAL BUSINESS DISTRICT DESIGN GUIDELINES

The West Capitol Avenue Design Guidelines apply to all parcels as indicated on the “Design Guideline Applicability Area” map in this document. Neighborhoods referenced in the requirements of this guideline are as indicated on the same diagram. Design Review consistent with the guidelines contained herein is required for all new construction on parcels within the applicability area, and on remodels or expansions where changes to the exterior finish, materials, structure or color are visible to public streets or other public areas, excepting window changeouts, re-roofs, repair of damaged building elements, parking lot restriping, installation of photovoltaic panels, interior TIs involving no exterior work, and similar projects.

Design Guideline Applicability Area



These Design Guidelines build, to a significant degree, upon previous documents including: the Strategic Plan 2006-2011-2020; the 1992 Action Plan for the West Capitol Corridor: Implementation Strategy; and the West Capitol Avenue Action Plan.

1. SITE DESIGN AND PLANNING OF THE PRIVATE REALM

The “private realm” consists of buildings and open spaces on individual privately-owned lots and parcels. It is necessary that there be ample freedom and flexibility in designing buildings in the private realm. However, there are certain features or aspects of building and site design that have a direct effect on the “public realm,” or the surrounding public context. The design guidelines presented here focus on the aspects of building design that are most likely to impact the overall character of the corridor.

These include:

1A Building Character

1B Parking

1C Service Areas and Access



2. SITE DESIGN AND PLANNING OF THE PUBLIC REALM

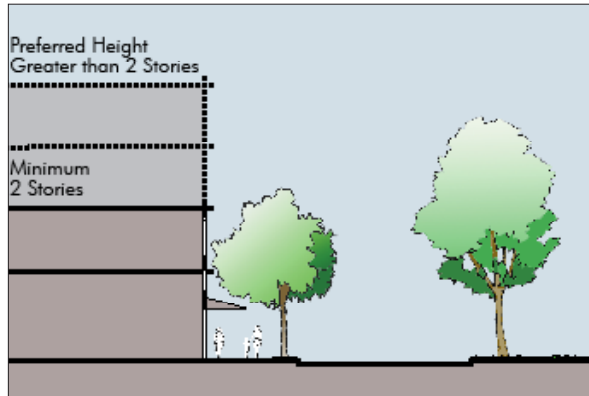
The intent of the design guidelines presented on the following pages is to reclaim the “public realm” for the pedestrian. The existing “public realm” is largely characterized by auto-oriented spaces and parking lots. The public area under discussion includes the right-of-way along West Capitol Avenue.

Improving the public realm will strengthen the character of the entire corridor. The development of the public realm is also the most effective way to create a pedestrian-friendly environment along West Capitol Avenue that will help bring life to the street.

Reclaiming the public realm will encourage greater opportunities for residents, users and visitors to make use of and enjoy the area. The design guidelines focus on various aspects of streets, including roadways, sidewalks, medians, lighting, etc.



1 Aii BUILDING HEIGHTS AND STEPBACKS



Minimum height of new development (1Aii-7)

1Aii-3

Allow development along Merkley Avenue east of Westacre Road (in the Downtown district) that is 45 feet in height.

1Aii-4

Development along Merkley Avenue west of Westacre Road should be 25 feet in height to respect the existing residential character along the south of Merkley Avenue. For buildings higher than two floors, provide upper story stepbacks at two stories for buildings that front along the north side of Merkley Avenue between Sycamore Avenue and Westacre Road. These stepbacks shall be no less than 10 feet.

1Aii-5

Provide development 45 feet in height along major north-south streets that have 60 to 80-foot rights of way (Popular Avenue, Westacre Road, Jefferson Boulevard, Harbor Boulevard). Upper story stepbacks shall be provided at four stories and above on streets that have 75 to 80-foot rights of way (Jefferson Boulevard, Harbor Boulevard).

1Aii-6

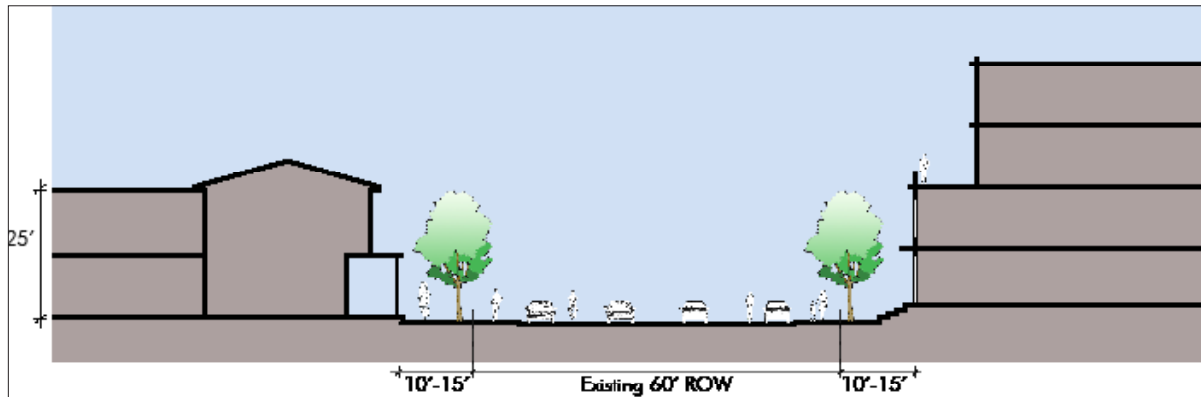
Development along local residential streets such as Pecan Street, Sycamore Avenue, etc shall be no more 20 to 25 feet in height to maintain the existing residential character.

1Aii-7

Building heights of two stories throughout the corridor help to create incentive for more intensive development along West Capitol Avenue.

1Aii-8

Encourage opportunities for increasing heights of single story buildings to help enhance the street definition.



25-foot building height with stepbacks for higher floors along Merkley Avenue

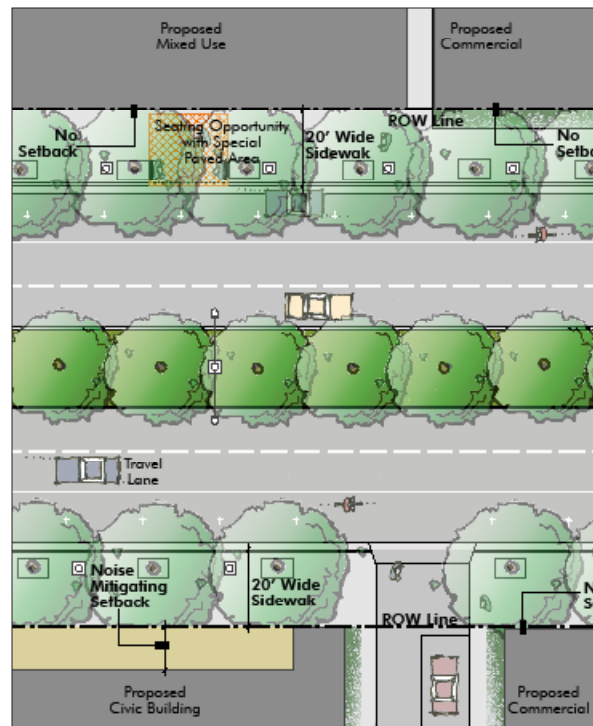
1 A III BUILDING SETBACKS

1Aiii-1

In the Downtown Core district with 20 feet wide sidewalks, buildings shall have no setback facing West Capitol Avenue. Create transitional open spaces such as café seating within the wide sidewalks.

1Aiii-2

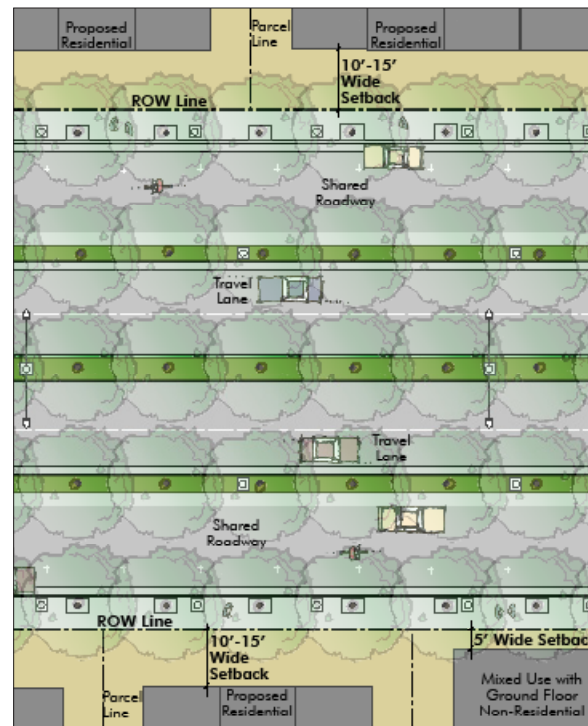
Provide exceptions to the above guideline for buildings with great civic importance, such as City Hall, and public/quasi-public uses that are sensitive to noise.



Downtown Core district building setbacks (1Aiii-1, 1Aiii-2)

1Aiii-3

In non-Downtown Core districts and along West Capitol Avenue, buildings shall be setback between ten and fifteen feet if the ground floor use is residential, and setback no more than five feet if the ground floor use is non-residential.

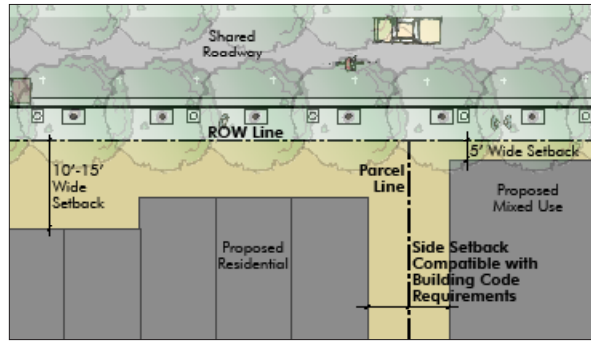


Non-Downtown Core districts building setbacks (1Aiii-3)

Setbacks, where required, provide opportunities for vibrant outdoor spaces, including café seating, plazas, front yards and porches. They also provide appropriate transitional spaces from public to private areas, increase privacy for individual buildings, and mitigate noise impacts. Setbacks vary depending on the sector, the need for buildings to provide street definition, the width of sidewalks and the kind of uses fronting the street.



1 A III BUILDING SETBACKS



Side building setbacks (1Aiii-5)

1Aiii-4

Permit zero lot line plotting for side and rear setbacks provided compliance with the building code is achieved.

1Aii-5

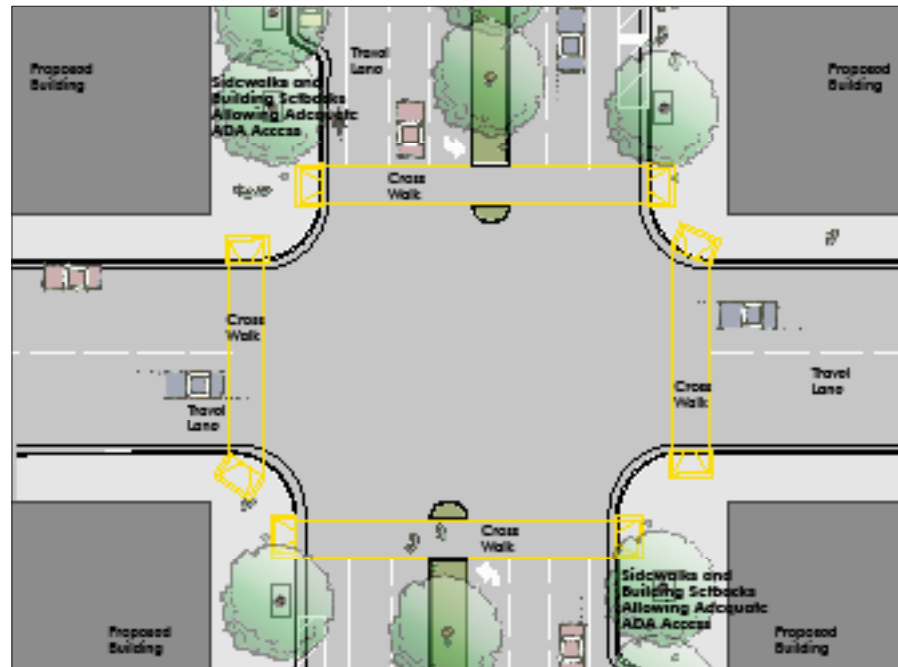
Side and rear setbacks shall be ten-feet side to adjacent residential zoned parcels to maintain light and shade into the adjacent properties.

1Aiii-6

Allow sidewalks and building setbacks at street intersections to meet the needs of the pedestrians and disabled community.

1Aiii-7

Buildings shall hold the corners to enhance the street definition whenever possible. Exceptions to this may be granted when doing so would conflict with the corner vision triangle requirements of the City's Landscape Development Guidelines.



Corner intersection setbacks for ADA access (1Aiii-6)



Corner intersection setbacks providing adequate visibility control area (1Aiii-7)

1 A I V B U I L D I N G U S E S

1Aiv-1

Provide mixed use development, preferably with ground floor retail, in the Downtown Core district.

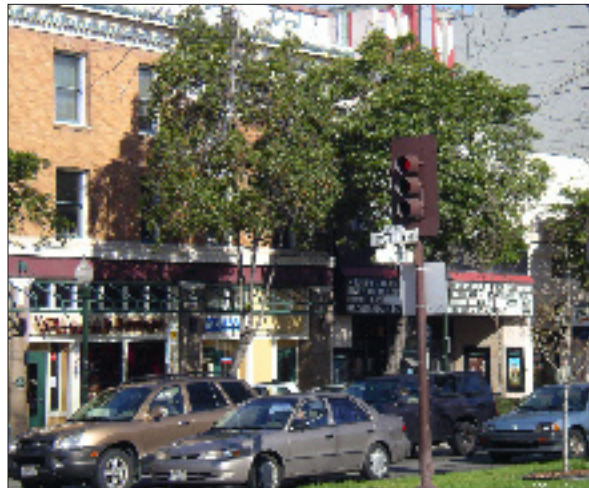
1Aiv-2

Retail and commercial uses shall be appropriately-scaled in non-Downtown Core districts.

1Aiv-3

Provide primarily residential uses in the Mixed Use Residential and Mid-Town Residential districts with non-residential uses at key areas and intersections.

The updated General Plan and Zoning Ordinance will determine the type of building uses. Recommended building use guidelines are based on: previous economic analyses that illustrate the viability of different uses; the type of existing and planned uses that help provide character to each Sector; and the overall West Capitol Avenue corridor.



Mixed use development with ground floor retail in Downtown

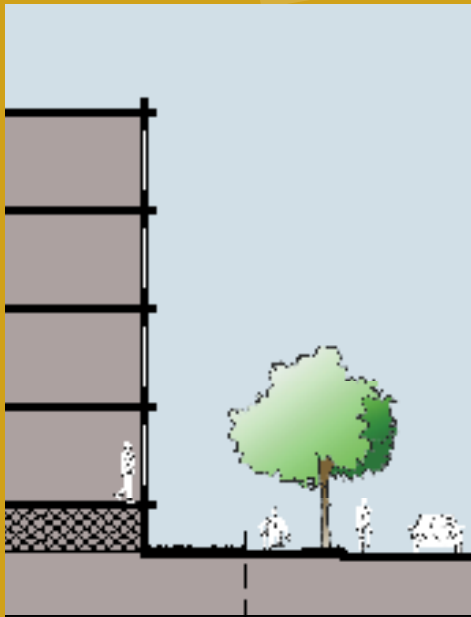


Primarily residential development in Residential districts

1 AV SITE LAYOUT AND BUILDING ORIENTATION

Building orientation should balance the goal of creating a strong active pedestrian edge along key streets with the need to provide seclusion to private open and built spaces, maximizing development and encouraging environmentally friendly buildings.

Building orientation should also seek to maximize the ability to provide green building opportunities such as passive solar, photovoltaic placement, and minimizing solar gain in the summer.



1Av-1

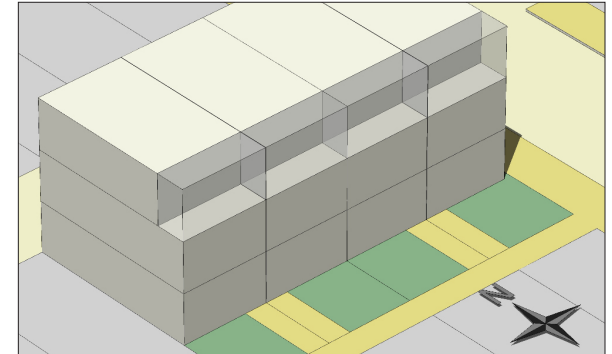
The primary building facades and entries for all uses, both commercial and residential, shall be directed towards West Capitol Avenue and other public edges.

1Av-2

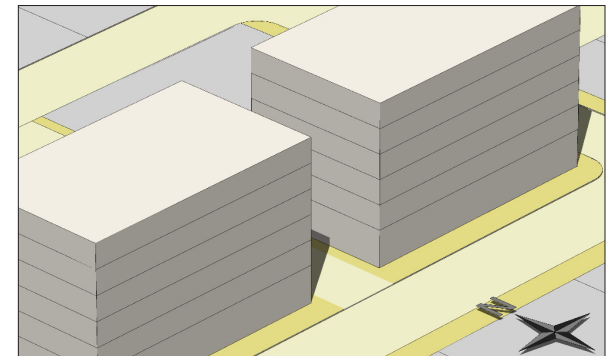
Buildings shall, to the extent feasible, be oriented to minimize exposure to the east, southeast, southwest and west sun.

1Av-3

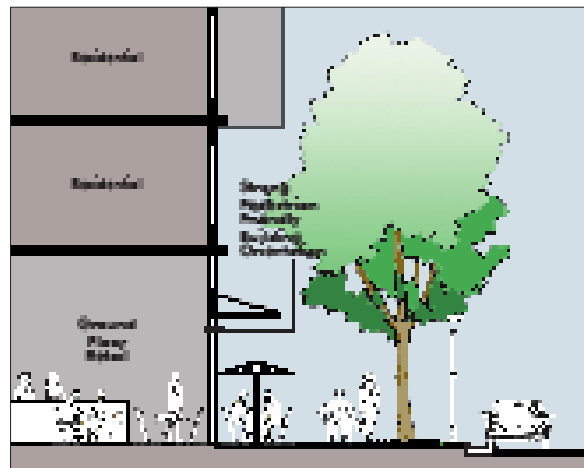
For mixed-use development in Downtown Core buildings, pedestrian-friendly retail, civic and commercial spaces shall be located on, but not necessarily exclusive to the ground/first floor.



North-south orientation for residential buildings



North-south orientation for commercial and mixed use buildings



Ground floor retail (1Av-3)

1 AV SITE LAYOUT AND BUILDING ORIENTATION

1Av-4

In case of uses such as residential and private office occupying the first floor of buildings in the Downtown Core district, provide privacy by elevating the first floor so as to be at least three feet above grade and allowing windowsills to be two to three feet above floor level.

1Av-5

In non-Downtown Core districts, quasi-private elements such as stoops, balconies, porches, stairs, etc, shall be located within the setback to further encourage privacy while enhancing the private open space realm and the adjoining public sidewalk.

1Av-6

Locate quasi-public residential spaces within buildings, such as living rooms along the building edge that front the streets, to maximize opportunities for “eyes on the street”.

1Av-7

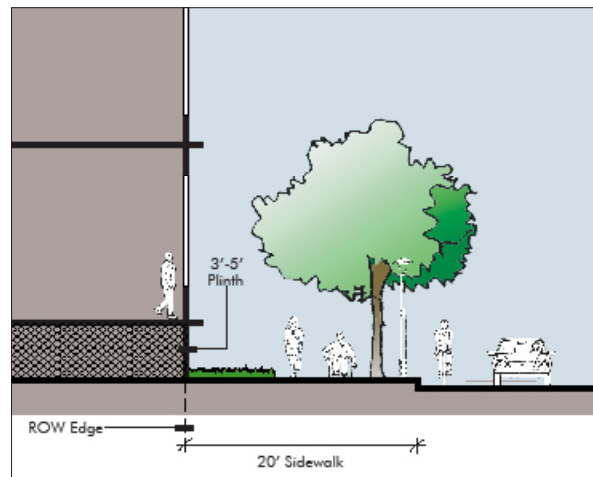
Explore new developments especially residential and mixed-use residential developments that are configured around open to sky courtyards that are contextually appropriate to the climate.

1Av-8

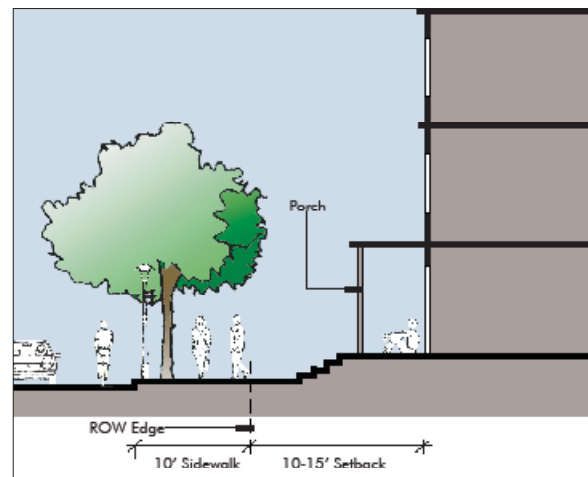
Common usable open spaces such as tot lots, greens and community gathering spaces shall be provided within multi-family residential development.



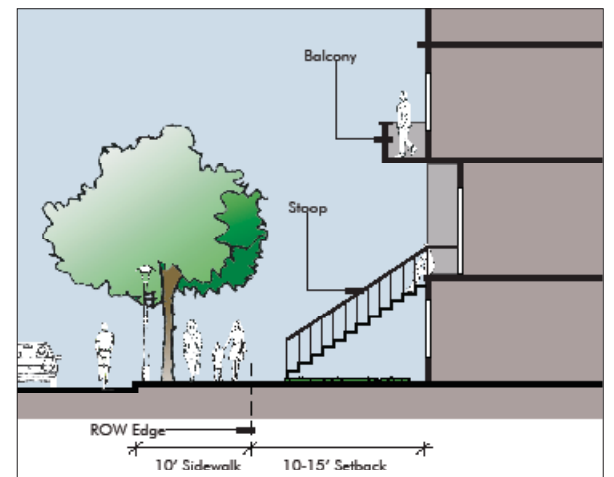
Common usable open spaces (1Av-8)



Elevated first floor of residential buildings (1Av-4)



Quasi-private elements within setback (1Av-5)



Quasi-private elements within setback (1Av-5)

1 AVI ADAPTIVE RE-USE



Adaptive re-use of motels into multi-family residential development

IAvi-1

As building needs change, encourage opportunities for intensifying existing single story buildings by adding additional floors or frontage along the street front.

IAvi-2

Provide opportunities for preservation of elements or features that support street life (e.g. pedestrian furniture near sidewalk), even as building use might change.

IAvi-3

Remodels / expansions and adaptive re-use of structures shall comply with all aspects of these guidelines except when determined to be infeasible by the Design Review Administrator



Intensified Land Uses



Adaptive reuse of motels

1 AVII BUILDING ARTICULATION

IAVII-1

Prioritize articulation of building facades facing West Capitol Avenue and other key pedestrian-friendly streets. Blank walls along street-fronting facades on any street are prohibited.

IAVII-2

Utilize building elements such as cornices, lintels, sills, balconies, awnings, porches, stoops, etc to enhance building facades.

IAVII-3

Vertical and horizontal articulation shall be used to mitigate long unbroken building elements.

IAVII-3

Colors shall be consistent with the overall design and building articulation. Multiple colors or shades and materials should be used in designs to help articulate the building form, and to provide interest at the pedestrian level.



Pedestrian-friendly building form



Well articulated building facades



Porches and stoops along guiding facades

1 AVII BUILDING ARTICULATION



Pedestrian friendly and well modulated architectural facade incorporating ecologically-friendly elements including photovoltaics, light shelves and recycled materials

Architectural Styles

IAvii-4

Buildings shall not employ specific past architectural styles with limited or no relevance to the plan area (mission style, etc.)

IAvii-5

Materials, forms and colors on buildings shall provide visual interest to the pedestrian level and contribute to the street edge. Material details and building articulation on a pedestrian scale include brick / masonry, wainscoting, wood details, metal cladding etc. The use of un-articulated stucco walls does not comply with this standard.

IAvii-6

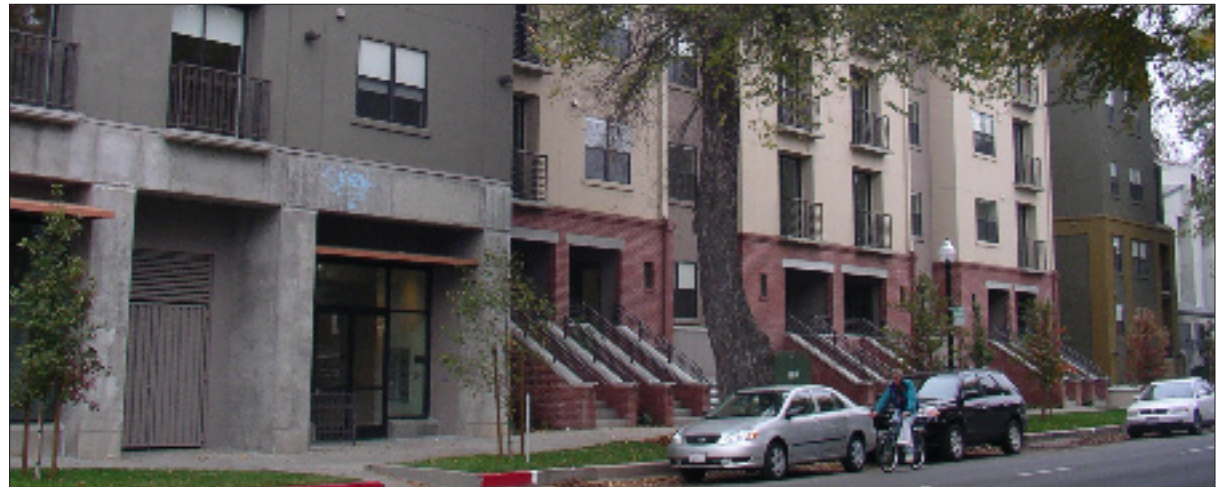
Employ ecologically conscious design solutions - in terms of styles, specific elements and materials - in the design of the architecture of buildings.

IAvii-7

Provide details and materials that are meaningful at a pedestrian scale, especially on first and second floor facades. (i.e. brick, wainscoting, windows, doors, stoops, awnings etc.)

IAvii-8

Low quality or undurable materials are prohibited in the plan area. This includes but is not limited to plywood siding, chain link fencing etc.



Building architecture with sufficient detail and variety of materials

1 AVII BUILDING ARTICULATION

Building Elements

1Avii-9

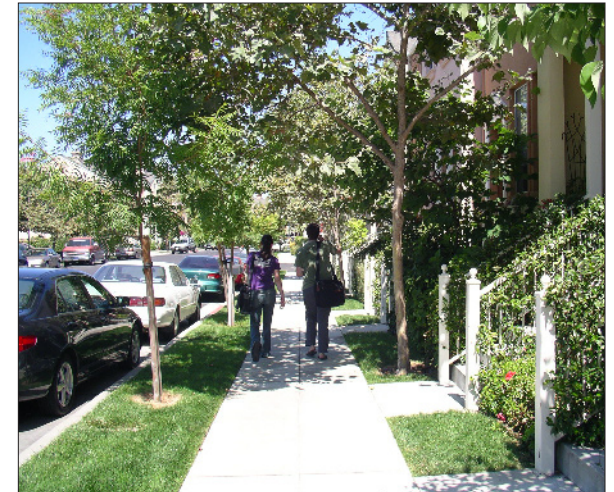
Provide awnings, balconies, bay windows and other such projections over public, 20 feet wide sidewalks in the Downtown Core district. Ensure an eight-foot height clearance for awnings from the finished sidewalk elevation.

1Avii-10

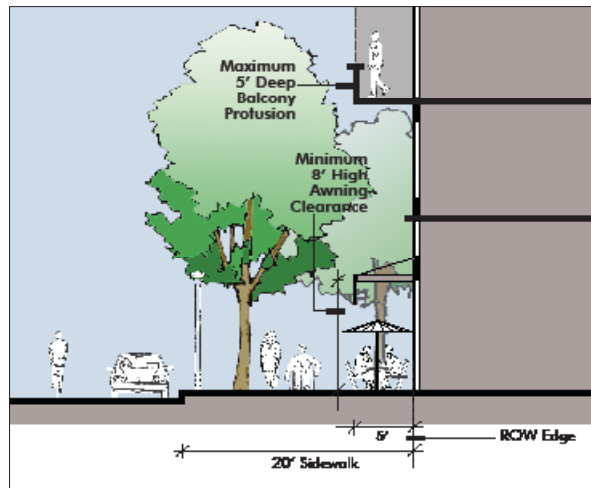
Projections may be up to five feet deep in the Downtown Core district.

1Avii-11

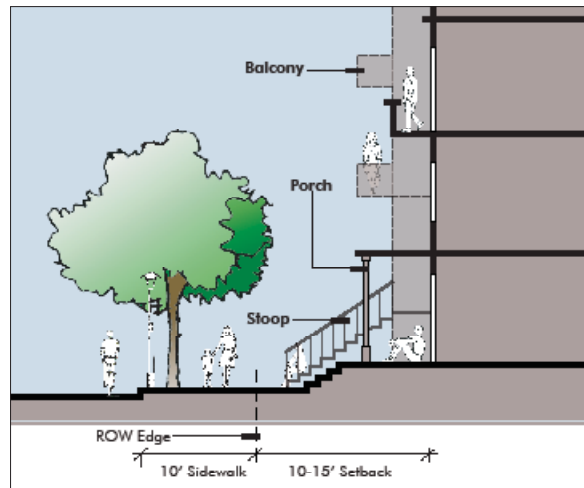
In non-Downtown Core districts, building elements such as porches, stoops, awnings, etc shall be provided within the five to fifteen-foot setback to enhance the relationship between the built environment and the street. It also increases pedestrian safety by providing additional opportunities for “eyes on the street.”



Stoops and porches within setbacks



Building Elements in Downtown Core (1Avii-9)



Building Elements in non-Downtown Core (1Avii-11)

1 AVII BUILDING ARTICULATION



Rhythmic fenestrations



Well articulated roof forms

Fenestrations (Doors and Windows)

IAvii-12

Primary pedestrian facades shall be marked by broad use of doors and windows to create an interesting pedestrian experience. Fenestrations shall be predominate elements on public facing facades.

IAvii-13

Frame fenestrations with protruding vertical and horizontal elements such as lintels, sills, etc to allow for the interaction of shadow and light. These building elements will also provide required protection from the elements (glare and rain).

IAvii-14

Encourage bold framing with deep recession or large protrusion around doors, windows and balconies.

IAvii-15

Require all ground floor retail and first floor office uses to have non-reflective glass windows fronting on West Capitol Avenue and other key pedestrian-friendly streets.

Roof Forms

IAvii-16 Mark key commercial and civic buildings with unique roof silhouettes to celebrate their importance and establish them as landmarks along the corridor.

IAvii-17

Building roof heights in large commercial buildings and major residential (attached townhomes, rowhouses, etc) development shall be varied to provide interest and variety in the building and to add value to the pedestrian environment.

IAvii-18

Articulate and accentuate roofs of key residential buildings, especially at street corners and entry of developments.

IAvii-19

Where possible, design roof forms to maximize their ability to hold solar panels.

IAvii-20

When visible to the pedestrian edge, roofing surfaces shall have a texture and material that provides adequate detail to be meaningful at the pedestrian scale including standing seam metal roofing, flat tile etc.. Composite roofing material is prohibited on roof elements that have line of sight visibility with a public edge. The use of mission style or terra cotta roof tiles is discouraged.

1 AVII BUILDING ARTICULATION

Lighting

1Avii-21

Buildings shall include the creative use of illumination to highlight architectural character of buildings and certain open space elements between buildings and sidewalks.

1Avii-22

In addition to public lighting, ensure entries are illuminated for safety. On site pedestrian areas shall be lit with pedestrian scale lighting such as building mounted fixtures or bollard lighting.

Mechanical Equipment

1Avii-23

All mechanical equipment, including ground, building and roof-mounted equipment, shall be screened from public view by landscaping and building architecture. This screening shall be consistent with the overall character and design of the building facades.

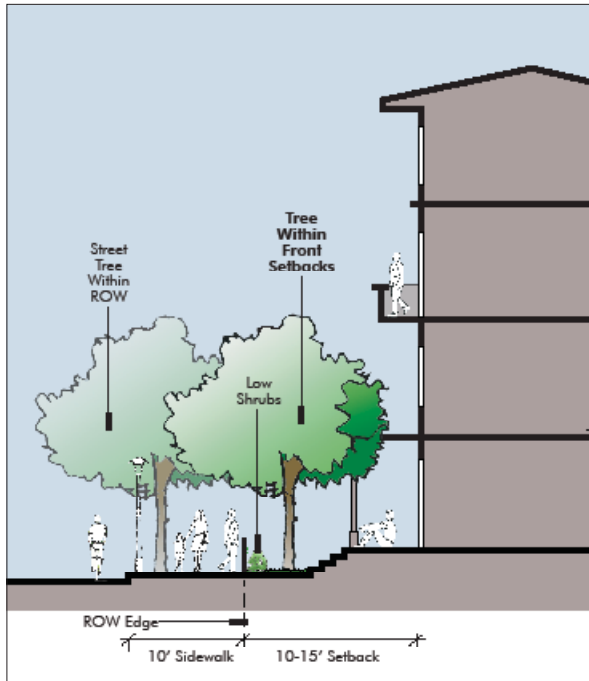


Creative lighting of buildings (1Avii-21)



Second floor trellis treatment screening the mechanical equipment (1Avii-24)

1 AVIII FRONT SETBACK ARTICULATION



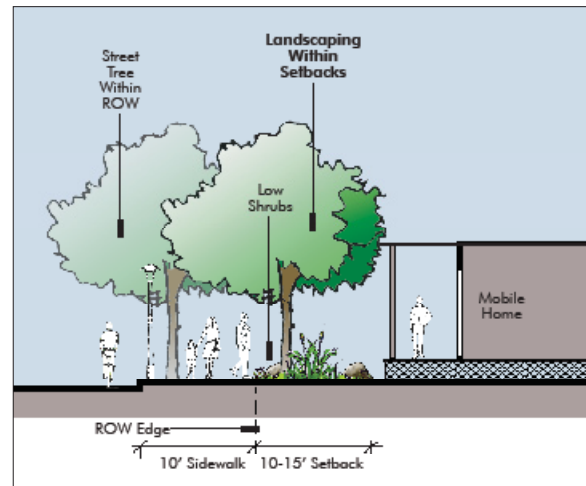
Trees and shrubs in non-Downtown Core districts (1Aviii-1)

1Aviii-1

In the non-Downtown Core districts, require planting of trees and shrubs within the ten to fifteen feet building setbacks along West Capitol Avenue.

1Aviii-2

Provide landscaped buffers (trees, shrubs, seating) between the back of sidewalks and existing residential mobile home parks.



Landscaping buffers to mobile home parks (1Aviii-2)

1Aviii-3

Mitigate existing blank walls along sidewalks by planting shrubs and trees. Murals may also be painted to minimize the harshness of blank walls.



Murals along blank walls (1Aviii-3)

1 AVIII FRONT SETBACK ARTICULATION

1Aviii-4

Fences and shrubs in front setback areas shall be no more than three feet tall. Exceptions may be made only for the screening of required building equipment/facilities.

1Aviii-5

Prohibit chain link, cyclone and similar types of fencing of any height in any street setback area.

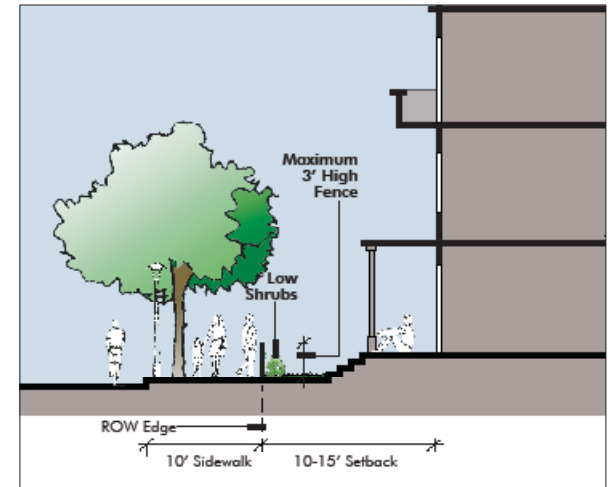
1Aviii-6

All landscaped areas shall be maintained, including proper trimming of plants growing into the public right-of-

way (tree branches, creeping vines, etc), consistent with approved plans and the City’s Landscape Development Guidelines.

1Aviii-7

Encourage building mounting and comprehensive inclusion of stealth wireless communication devices in a manner that is consistent with the overall building architecture and design.



Low fences and shrubs (1Aviii-4)



1 AIX CONSTRUCTION MATERIALS



Solar panels as facade materials

1Aix-1

Re-used and recycled materials should be highlighted and featured in designs, the plan area strongly encourages green building strategies and LEED design principles.

1Aix-2

Encourage energy-efficient design of new buildings and the retrofitting of existing buildings to reduce energy consumption (including awnings, courtyard style housing and other solutions to mitigate direct sunlight in the summer).

1Aix-3

Permeable paving elements shall be used in non-auto-oriented areas, and implemented within auto oriented areas where possible.



Permeable paving materials (1Aix-3)



Recycled wood for window siding



1 A X S I G N A G E

1Ax-1

Provide signage and other features on buildings that are pedestrian-oriented and scaled so as to better communicate with people on the street and to enhance building character.

1Ax-2

Allow signage that can be wall mounted or suspended from awnings above the sidewalk. This signage shall be in a scale and style focused on the pedestrian user.

1Ax-3

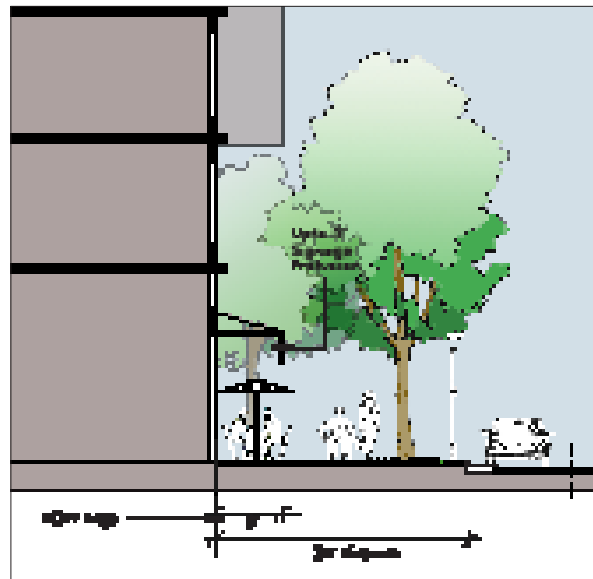
Allow signage to protrude up to five feet deep in the public right of way of the Downtown Core district. Allow signage in non-Downtown Core districts to encroach into the public right of way by one foot.

1Ax-4

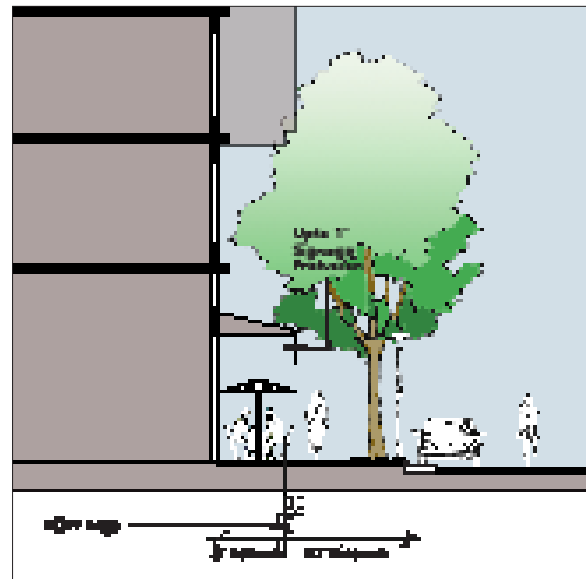
Provide signage that is consistent with the building vocabulary. Signage for known tenants shall be included in with design review materials. If tenants are not known then a sign program shall be proposed.



Signage mounted on awnings (1Ax-2)



Signage in Downtown Core district (1Ax-3)



Signage in non-Downtown Core districts (1Ax-3)

1 B I PARKING SUPPLY

The updated General Plan will determine the final parking requirements for West Capitol Avenue. Where possible, parking needs should not minimize the pedestrian-friendly edges of buildings. These recommended requirements build upon the desired multi-modal, transit-friendly future of the corridor and the need to have a pedestrian and bike-friendly image of the corridor.

1Bi-1

Allow the use of shared parking where spaces are provided in lots that are shared with other buildings, especially if the building uses have different peak-demand time periods, as allowed by the zoning ordinance or specific plan.

1Bi-2

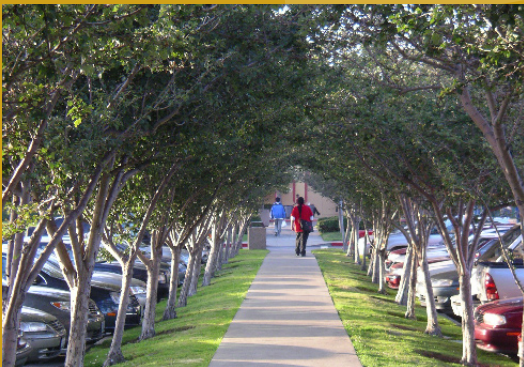
Portions of parking requirements of individual projects may be satisfied by on-street parking where allowed for by the zoning ordinance or specific plan.

1Bi-3

Provide opportunities for developers to unbundle parking to allow residents to choose whether or not they rent and/or own their own parking space.

1Bi-4

Individual residential garages and covered parking are prohibited from taking direct access to West Capitol Avenue.



On-street parking to meet overall parking demands (1Bi-2)

1 B I I O N - S I T E S U R F A C E P A R K I N G

1Bii-1

All parking in new developments shall be located behind residential and commercial structures along West Capitol Avenue.

1Bii-2

Where parking is not possible at the rear of the building, provide parking at the side.

1Bii-3

Mitigate the pedestrian-unfriendly character of existing parking lots that have cars fronting the edge of the right of way with a five-foot setback that contains improvements such as a row of trees, low shrubs and grasses, landscaped pergola, etc.

1Bii-4

Lighting, trees and other landscaping within parking lots shall be consistent with and maintained in accordance to the City of West Sacramento Landscape Development Standards, and approved plans.

Parking should be accessible and visible to all users. However, the location of parking and the edge of parking areas should not detract from the pedestrian-friendly street life along West Capitol Avenue.



Parking lot edge articulation (1Bii-4)

1 BII ON-SITE SURFACE PARKING



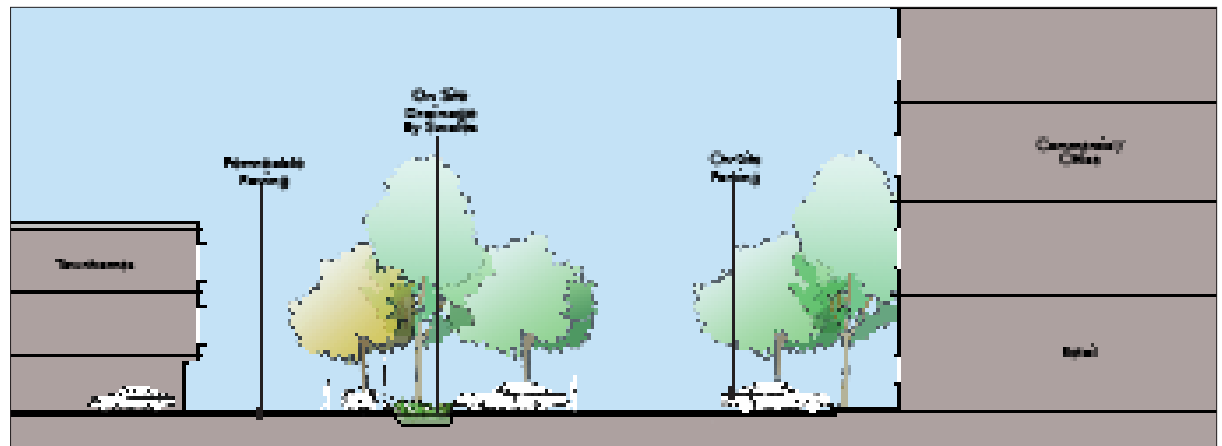
Swales adjoining parking lots (1Bii-6)

1Bii-6

Parking areas should attenuate run-off from existing and planned parking lots with options such as permeable paving and landscaped swales. If needed, provide setbacks greater than five feet for swales adjoining the back of sidewalks. Permeable paving options such as pavers etc. shall be used in locations clear of emergency vehicle paths of travel.

1Bii-7

Design automobile ingress and egress to minimize conflicts with pedestrian movement and achieve the curb cut requirements detailed in the Site Design and Planning of the Public Realm section of this chapter.



Swales to attenuate parking and surface run-off (1Bii-6)

1 B III PARKING GARAGES

1Biii-1

Garages shall have pedestrian-friendly ground floor uses such as cafes, small shops, etc to front on West Capitol Avenue and other public edges. Garage elevations that face public streets shall be articulated with a level of detail consistent with other building types.



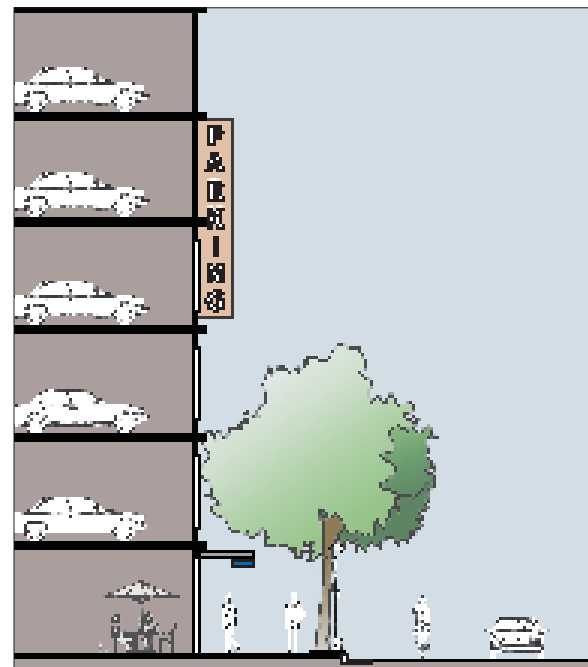
Pedestrian-friendly ground floor uses for garages (1Biii-1)

1Biii-2

Articulate vertical elements—such as elevators, stairwells and attractive signage—to be visually prominent and orient these elements along West Capitol Avenue and key pedestrian corridors.

1Biii-3

Provide appropriate signage to parking garages that are visible to automobiles, provide consistency with the building vocabulary, and add interest and variety to the pedestrian edge.



Articulation of vertical elements (1Biii-2)

As intensity of development along West Capitol Avenue corridor increases, the need to have parking garages will also increase. While parking garages should be efficiently laid out, the interface between the public and private realms of these buildings should respect the overall street life.



1 C I SERVICE AREAS AND ACCESS



Screening of outdoor trash containers (1Ci-1)

1Ci-1

All outdoor trash and garbage containers shall be located away from the public view. These enclosures shall be screened with landscape elements or walls that match the design of the primary structure, and designed in conformance with City trash enclosure standards.

1Ci-2

Locate loading and unloading areas of commercial buildings, especially grocery stores, to minimize the noise impacts to adjoining properties.

1Ci-3

Where properties front on two arterials, service areas shall be located along the side, not along the street frontage, of the property.

1Ci-4

Design buildings and service area access to minimize ingress and egress conflicts with pedestrians.

1Ci-5

Encourage service access to make use of alleys where they exist. Create new internal streets/alleys where needed.



Service access of grocery stores (1Ci-2)

1 C I SERVICE AREAS AND ACCESS

1Ci-6

Provide minimum 20 feet wide service access internal street/alley for residential townhomes, rowhouses, multi-family residential units, etc.

1Ci-7

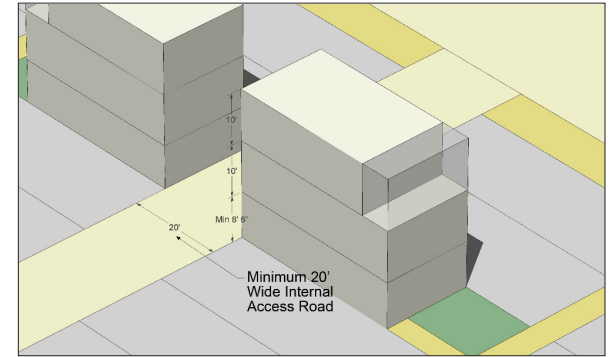
Two-way internal streets for commercial complexes shall be 25 feet wide.

1Ci-8

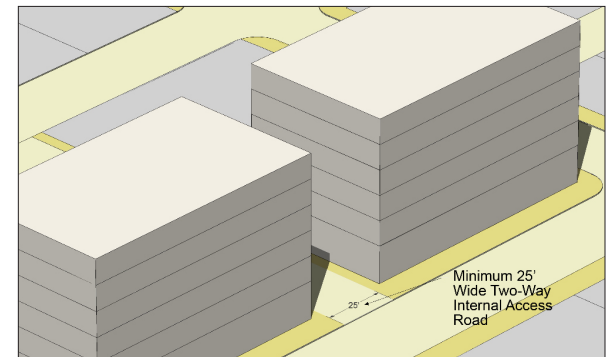
One entry is permitted per development. Where more than one access point is needed, there should be at least 150 feet between two driveways.

1Ci-9

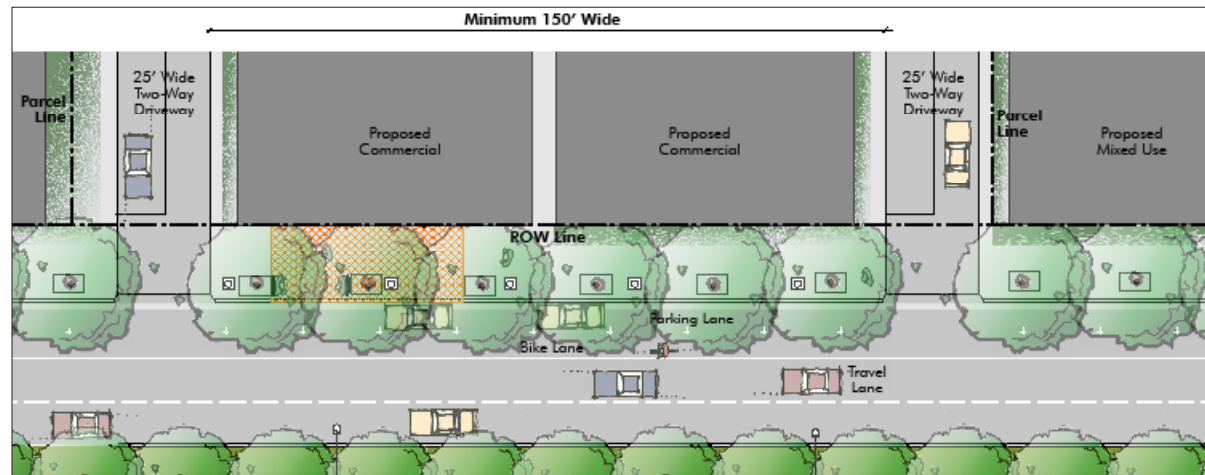
Allow additional access to gas stations due to their special needs.



Minimum 20-foot-wide residential access road (1Ci-6)

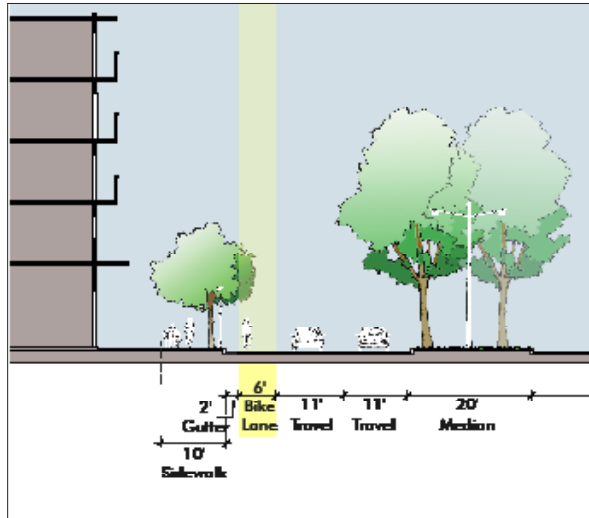


Minimum 25-foot-wide non-residential access road (1Ci-7)



Access points to developments (1Ci-8)

2 A I ROADWAYS



Bicycle lanes adjacent to curbs (2Ai-2)

2Ai-1

All travel lanes shall be a minimum of eleven feet in width.

2Ai-2

Bicycle lanes adjacent to parking lanes shall be seven feet in width and bicycle lanes adjacent to curbs shall be six feet in width (excluding gutter).

2Ai-3

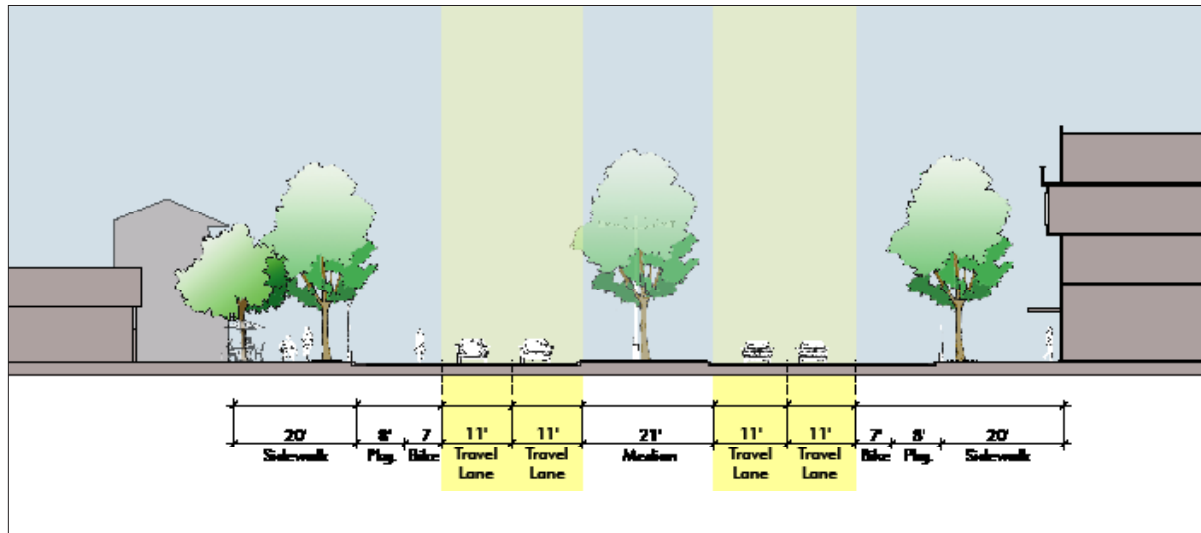
Ensure parking lanes in the Downtown Core and East Gateway districts are eight feet in width, and seven feet in width on access lanes within multi-way boulevards between Westacre Road and Sycamore Avenue.

2Aiv-4

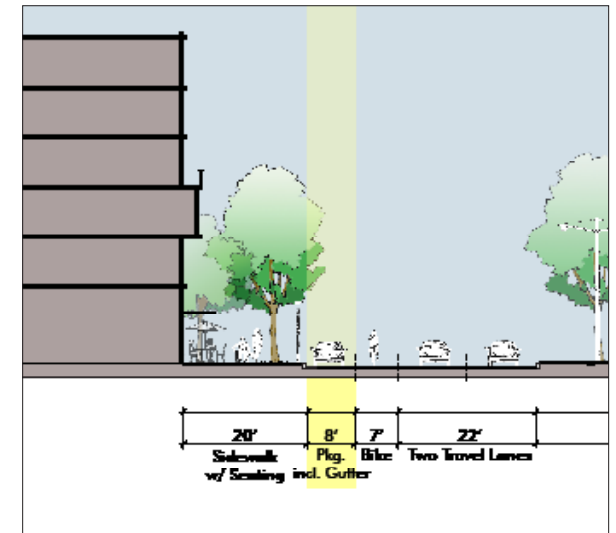
Provide curb parking at key civic destinations, including the library and post office.

2Aiv-5

Prohibit truck parking via ordinance and enforcement. Design options to make truck parking inconvenient, such as markings, tree wells, plant boxes etc. within parking spaces should be used where possible.



Travel lanes (2Ai-1)



Parking lanes (2Ai-3)

2 A I ROADWAYS

2Ai-7

Provide bus stop lanes at least ten feet wide.

2Ai-8

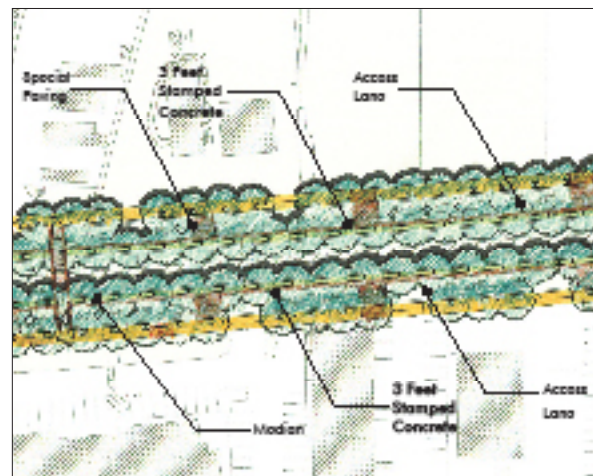
Design roadways to be at least 20 feet in width to allow emergency access (fire, police and ambulance) throughout the corridor.

2Ai-9

Design access roadways in multi-way boulevards (between Westacre Road and Glide Avenue) to be shared by bicycles and automobiles.

2Ai-10

In Mid-Town Residential and Mixed Use Residential districts, place a three-foot section of stamped concrete on the median next to the access roadway with color and



Stamped concrete in access roadway medians (2Ai-10)

texture that discourages driving except by emergency vehicles.

2Ai-11

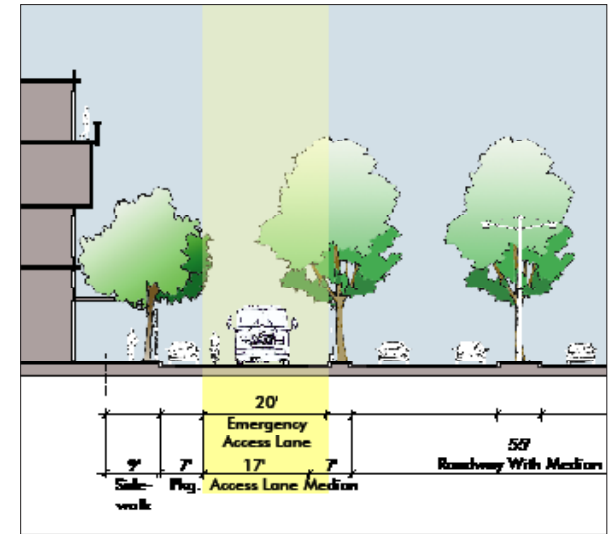
Allow a mid-block U-turn for cars and emergency (fire, police and ambulance) vehicles for each of the blocks between Merkeley Avenue and Sycamore Avenue. Design these turns to be flush with the roadway.

2Ai-12

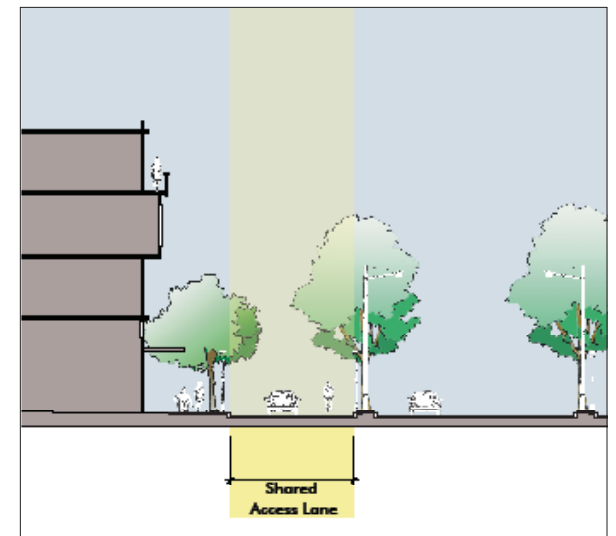
Allow a mid-block U-turn for emergency vehicles (fire, police and ambulance) vehicles for each of the blocks between Garden Street and Merkeley Avenue and between Glide Avenue and Harbor Boulevard. Design these turns to be flush with roadway.

2Ai-13

Allow a special mid-block left turn to access El Rancho Mobile Home Park for emergency vehicle access only.

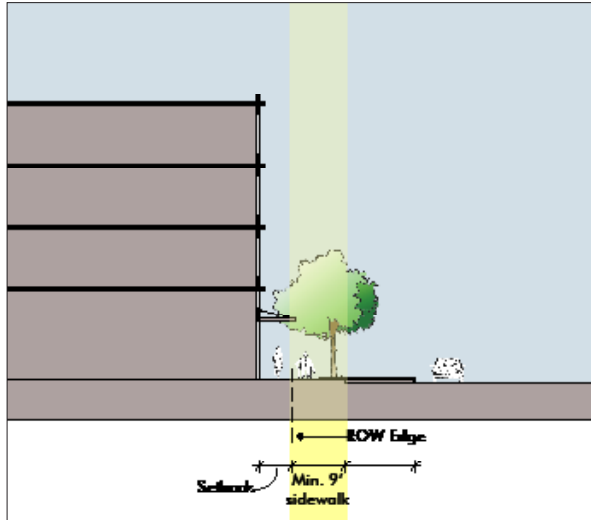


Emergency access lanes (2Ai-8)



Shared access lane (2Ai-9)

2 A II SIDEWALKS



Minimum sidewalk width (2Aii-2)

2Aii-1

Sidewalks are required on both sides of all streets.

2Aii-2

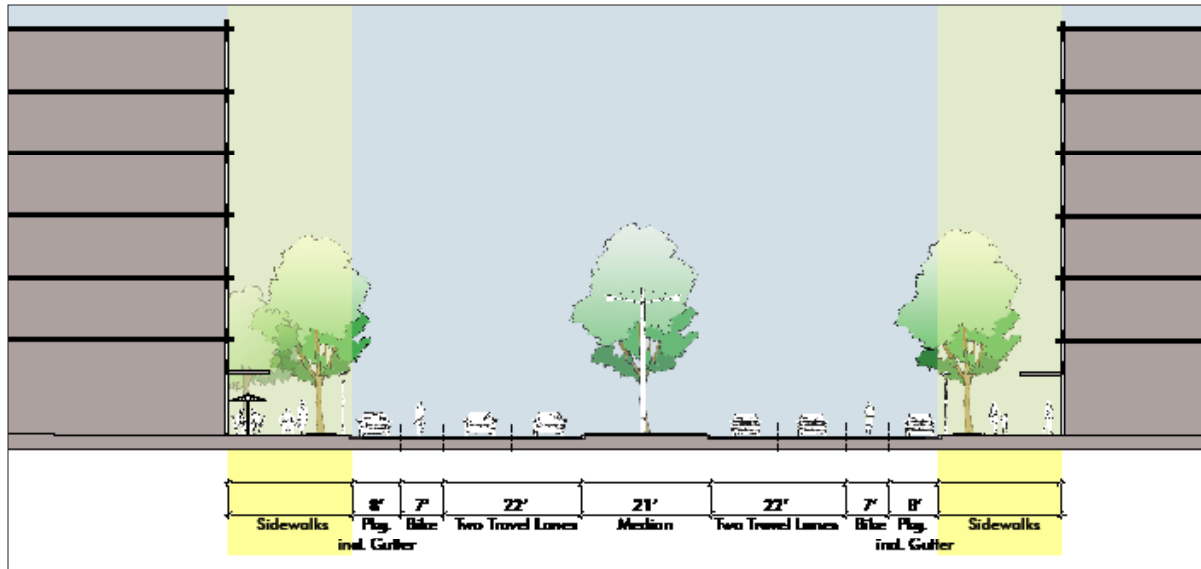
Ensure that all sidewalks are at least nine feet wide.

2Aii-3

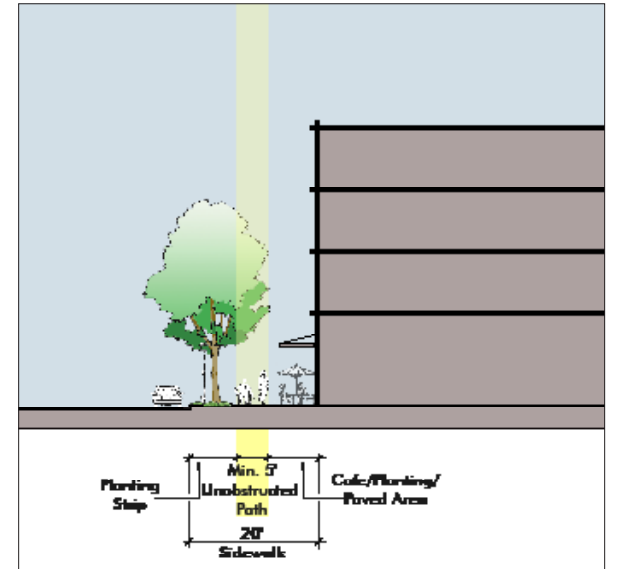
Require a clear, contiguous, and unobstructed path that is five feet wide for ADA access.

2Aii-4

Sidewalks shall be 10 feet wide in the East Gateway, West Gateway and Mixed Use Residential districts.



Sidewalks on both sides of street (2Aii-1)



Unobstructed ADA access (2Aii-3)

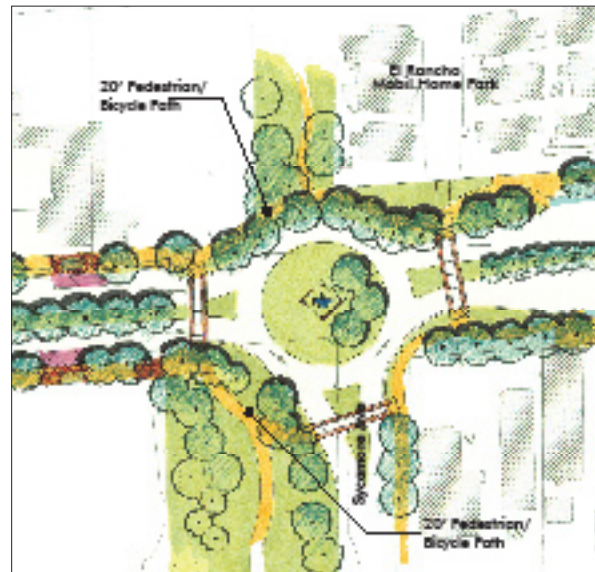
2 A II SIDEWALKS

2Aii-5

Ensure the shared pedestrian/bicycle path at Sycamore Avenue is ten feet wide to allow sufficient room for both modes.

2Aii-6

Widen sidewalk in front of City Hall to allow for a pedestrian-friendly plaza.



Shared pedestrian/bicycle path (2Aii-5)

2Aii-7

In the Downtown Core district, allow outdoor seating for restaurants and cafes provided there is a minimum seven feet wide sidewalk clearance for pedestrians.

2Aii-8

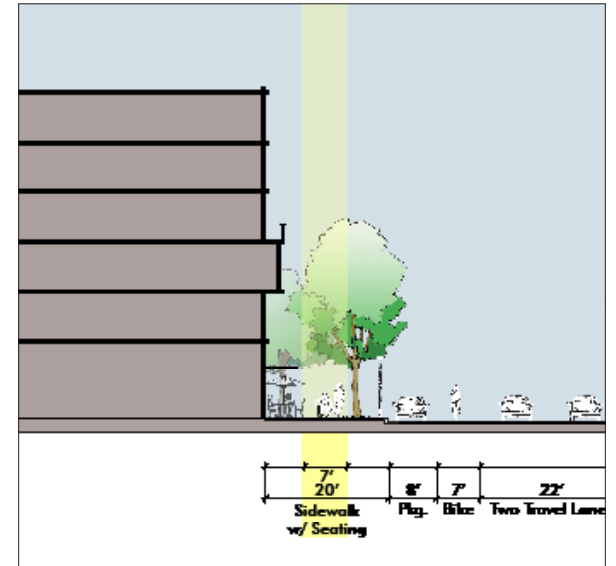
Paved pedestrian paths shall be required from sidewalks to entrances of new developments.

2Aii-9

Create similar pedestrian paths where needed in existing development.

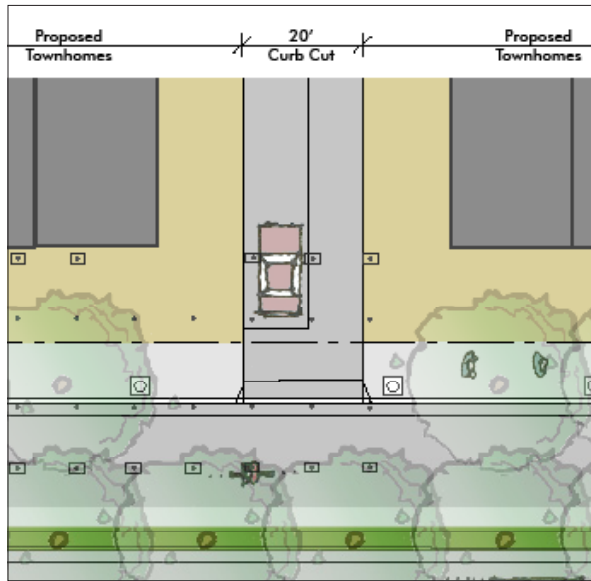


Widened sidewalk at City Hall (2Aii-6)

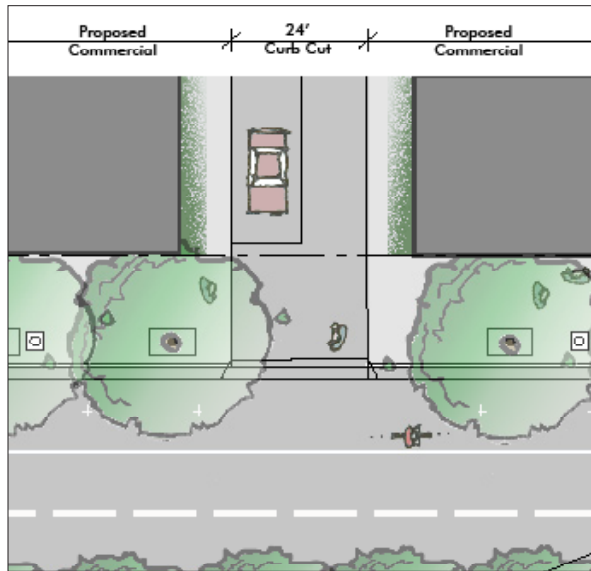


Minimum sidewalk clearance in Downtown Core (2Aii-7)

2 A III CURB CUTS



Residential two way entries/exits (2Aiii-2)



Non-residential two way entries/exits (2Aiii-2)

2Aiii-1

Curb cuts shall be minimized to lessen the impact on pedestrian safety and overall quality of the pedestrian environment.

2Aiii-2

New commercial uses, except gas stations, shall have two-way entries/exits that are no more than 20 feet in width in residential districts and 24 feet in width elsewhere. For paired one-way entrances, ensure that each entry is no more than 12 feet in width.

2Aiii-3

Special consideration should be made for life safety operations, including police and fire.



Pedestrian-friendly curb cuts

2 A I V C R O S S W A L K S

2Aiv-1

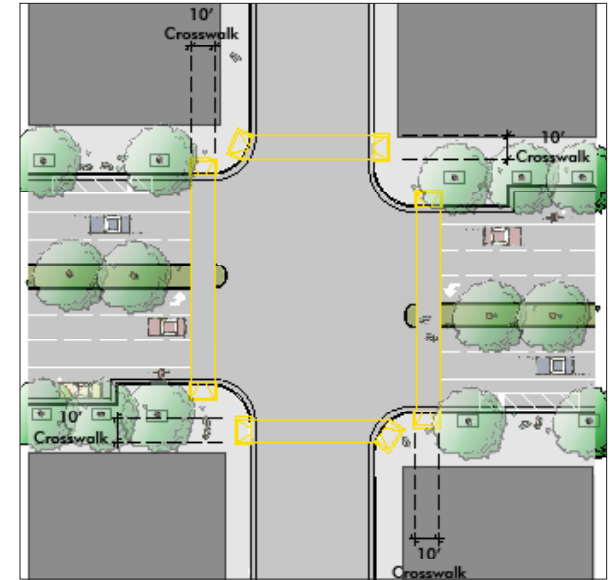
Provide clearly marked crosswalks, which are at least ten feet in width and have ramps for ADA access, at all controlled intersections.

2Aiv-2

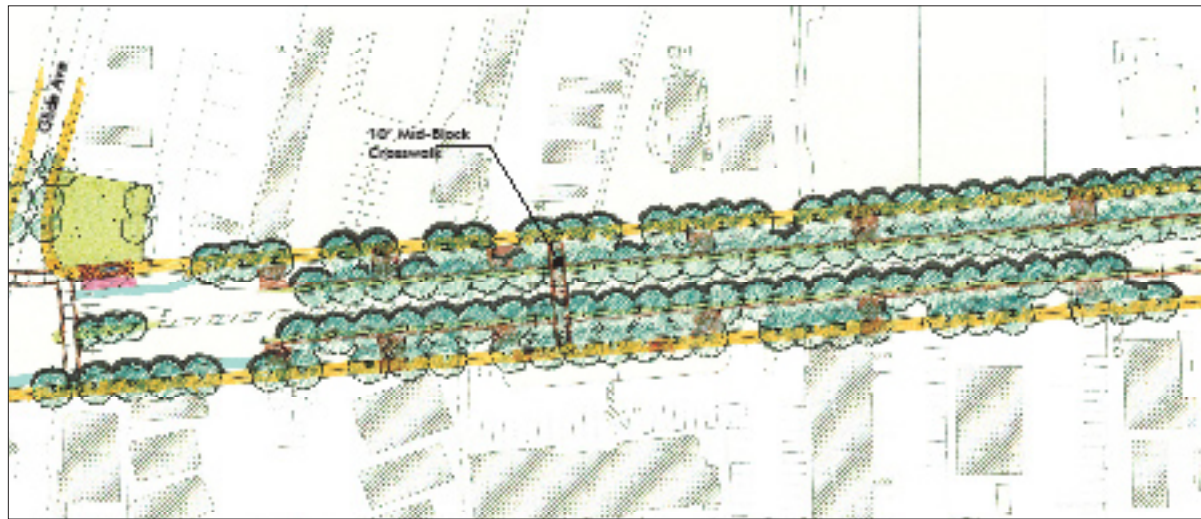
Mid-block crossings shall be highly visible and a minimum of ten feet wide.

2Aiv-3

Provide crosswalks that are a minimum of 20 feet wide in the Downtown Core district.



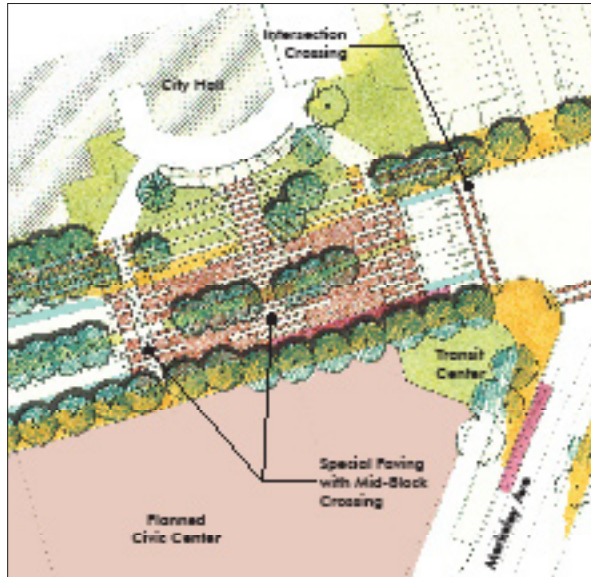
Controlled intersection crosswalks (2Aiv-1)



Mid-block crosswalks (2Aiv-2)



2 A I V C R O S S W A L K S



Civic Center crosswalk (2Aiv-4)

2Aiv-4

Design the Civic Center crosswalk to be wider than other mid-block crossings; design to be 25 feet in width.

2Aiv-5

Provide refuge for pedestrians in the medians at key intersections.

2Aiv-6

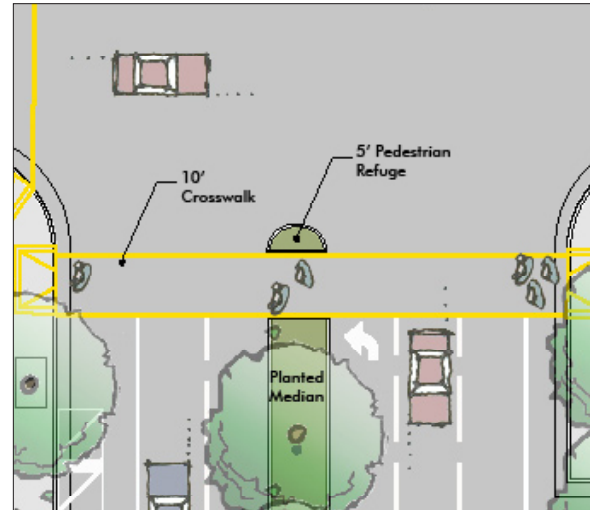
Bulb-outs should be provided at all intersections to minimize crossing distance and increase pedestrian visibility.

2Aiv-7

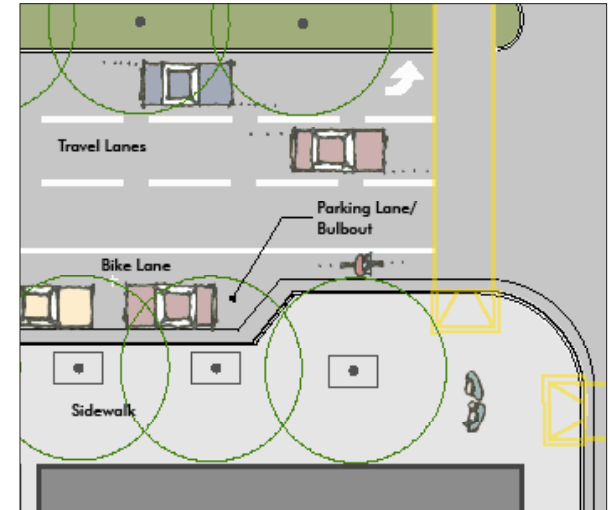
Design crosswalks with a smooth, slip-resistant path of travel in the center of the crosswalk for sensitivity to certain ADA conditions, while paving the remaining areas of the crosswalk with colored and textured materials, for visibility and visual interest.



Pedestrian refuge at intersections (2Aiv-5)



Pedestrian refuge at intersections (2Aiv-5)



Bulb-outs at intersections (2Aiv-6)

2 A V M E D I A N S

2Av-1

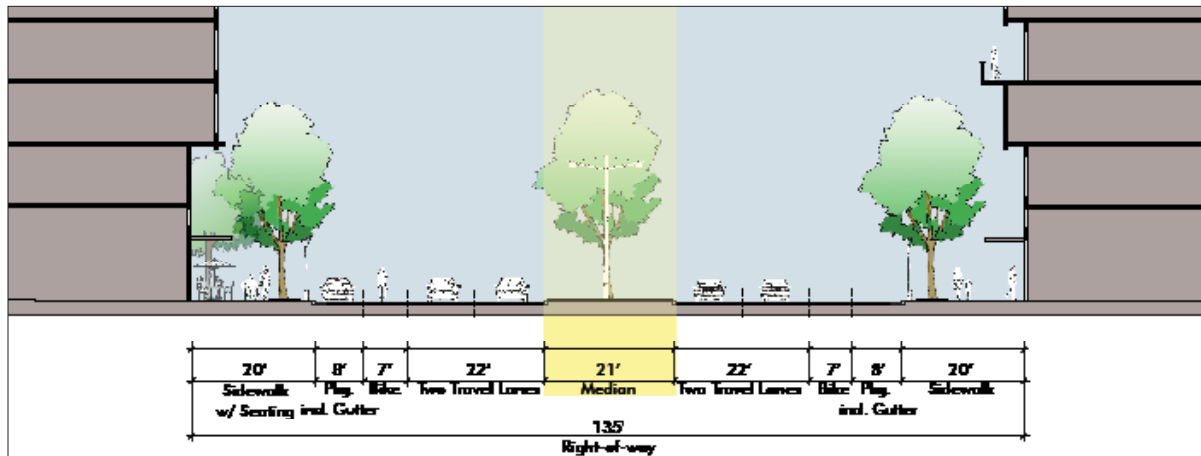
Central and side landscaped medians shall be provided in accordance with approved streetscape improvement plans.

2Av-2

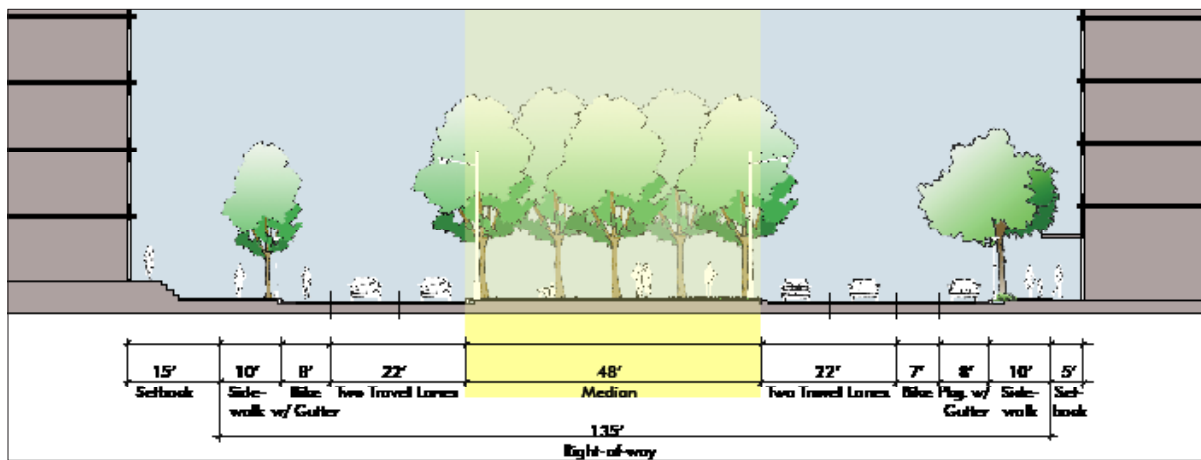
Provide an approximately 21 feet wide central median in the Downtown Core district and a 20 feet wide central median in the West Gateway sector. Provide a large 48 feet wide median in the East Gateway.



Central landscaped medians



Downtown Core median (2Av-2)



East Gateway median (2Av-2)

2 AV MEDIANS



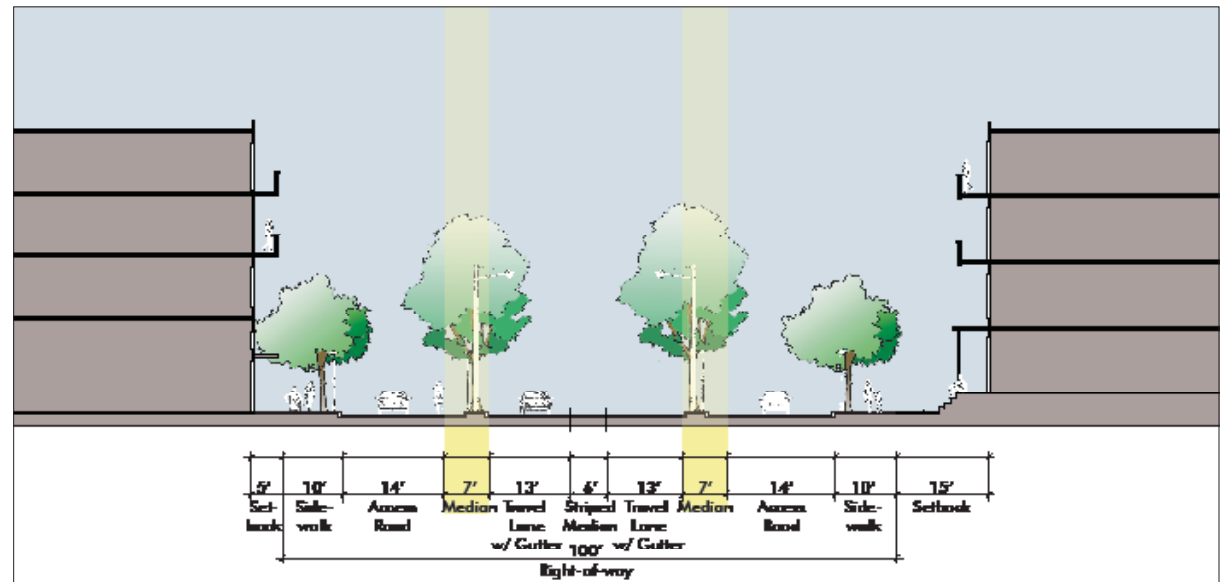
Side landscaped medians

2Av-3

Provide approximately seven foot wide side medians in the Mixed Use Residential and Mid-Town Residential districts, with four feet of landscaping and three feet of special paving for use only by emergency vehicles.

2Av-4

Allow for transit stops for future light rail/trolley within central medians that are more than eighteen feet wide.



Side medians in Mixed Use and Mid-Town Residential districts (2Av-3)

2 AVI LANDSCAPING OF SIDEWALKS AND MEDIANS

2Avi-1

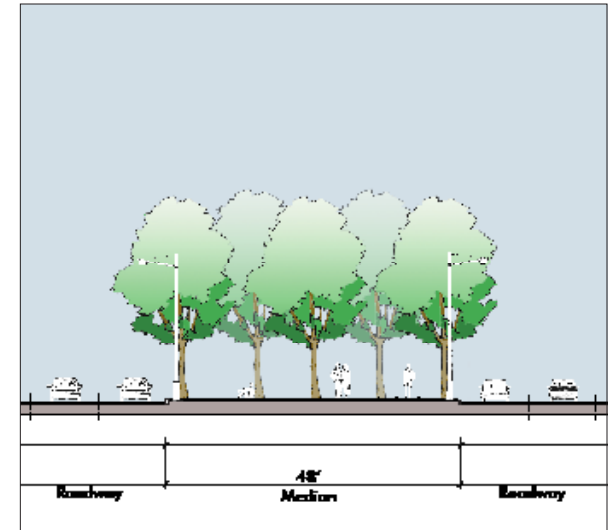
Street trees shall be planted approximately 30 feet apart on-center in the Downtown Core district. Tree wells shall be five feet by eight feet in size.

2Avi-2

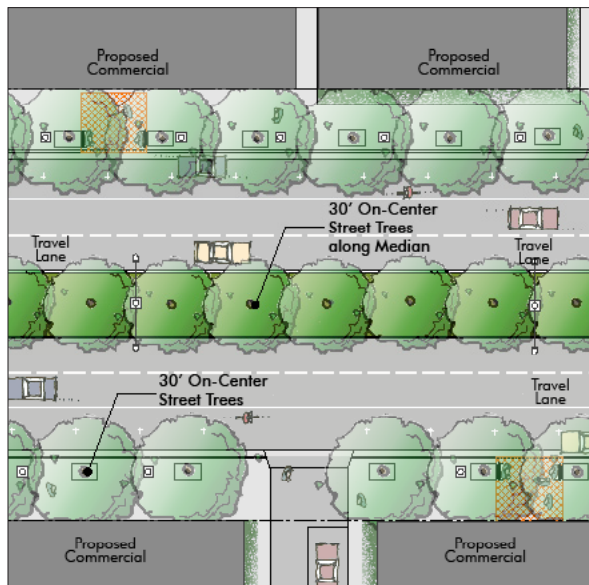
Street trees shall be planted approximately 24 feet apart on-center in the non-Downtown Core districts. Tree wells shall be four feet by six feet in size.

2Avi-3

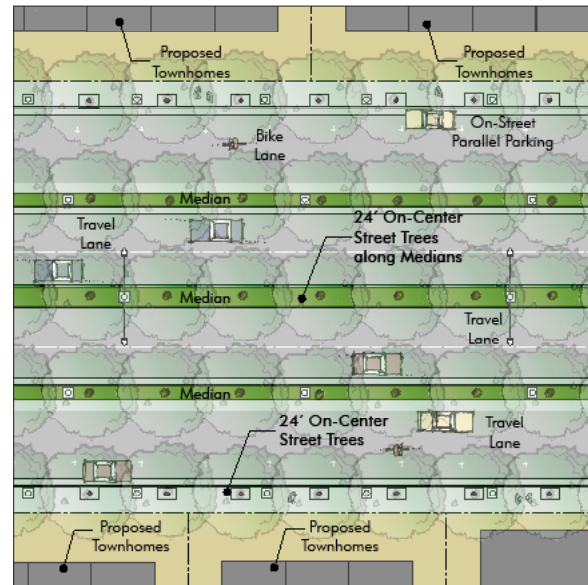
Allow for ground turf along the central median in the East Gateway district, in accordance with approved streetscape improvement plans, to provide opportunities for people to enjoy the open space.



East Gateway turf median (2Avi-3)

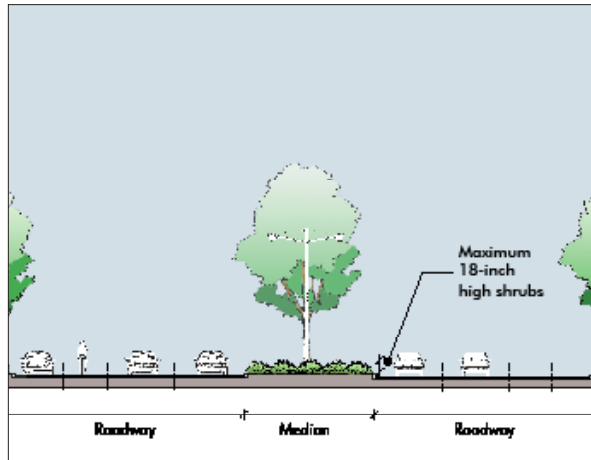


Downtown Core median street trees (2Avi-1)



Non-Downtown Core median street trees (2Avi-2)

2 AVI LANDSCAPING OF SIDEWALKS AND MEDIANS



Shrubs in median (2Av-4)

2Avi-4

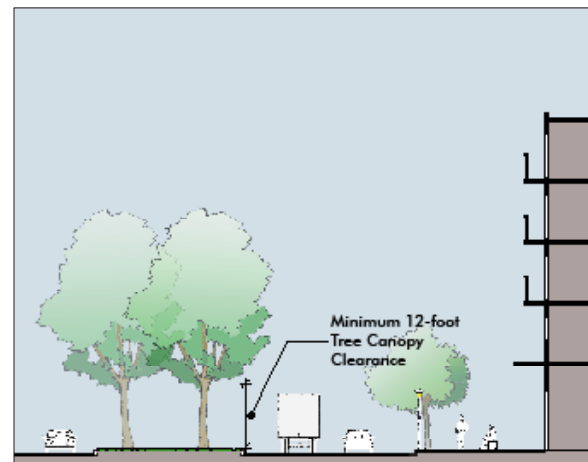
Maintain low height shrubs not more than eighteen inches high in the central and side medians of districts.

2Avi-5

Plantings, such as London Plane (Sycamore), should emphasize the civic stature and character of the Downtown Core district, while plantings, such as fruit trees, in the Mixed Use Residential districts should contribute to a more residential character. All street tree plantings shall be in accordance with the approved streetscape improvement plans, and of a species approved by the City Arborist.

2Avi-6

Street trees shall be maintained with at least twelve feet tree canopy clearance from the finished sidewalk elevation to provide clear emergency and service access, to not block light from the pedestrian-scaled lighting, and to allow for a visual connection along the sidewalks and medians.



Tree canopy clearance (2Av-6)



2 AVII STREET LIGHTING

2Avii-1

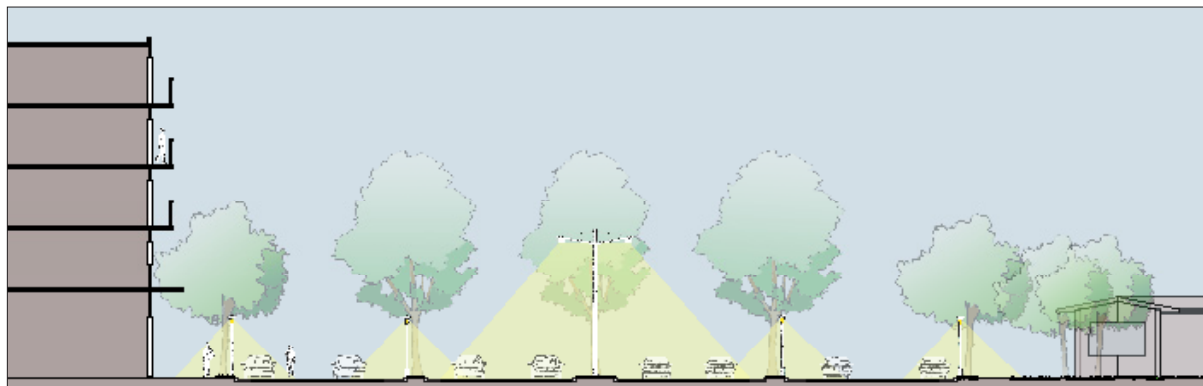
Provide street lighting at two different levels: 1) Pedestrian-oriented lighting that illuminates the sidewalk and part of the adjoining street (that usually includes the parking and bicycle lane); and 2) Automobile traffic-oriented lighting that is centrally placed at a higher height.

2Avii-2

Require pedestrian lighting to be at lower height (approximately twelve feet high), spaced closer, and use full spectrum bulbs. Ensure pedestrian lighting along all sidewalks and installed in accordance with approved streetscape improvement plans and City standards.



Scale of street lighting (2Avii-1)



Lighting of multi-way boulevard (2Avii-2)

2 AVII STREET LIGHTING



Auto-oriented street lighting (2Avii-3)

2Avii-3

Require automobile traffic-oriented lighting to be higher, further-spaced and centrally located within medians. All street lighting shall be installed in accordance with the approved streetscape improvement plans.

2Avii-4

Ensure the spacing and type of street lights meet the illumination standards of the City.



Different scales of street lights

2 AVIII STREET FURNITURE

2Aviii-1

Provide low maintenance street furniture elements that include seating, garbage cans, newspaper stands, etc.

2viii-2

Locate seating on sidewalks adjacent to major destinations, major mobile home parks, etc.

2viii-3

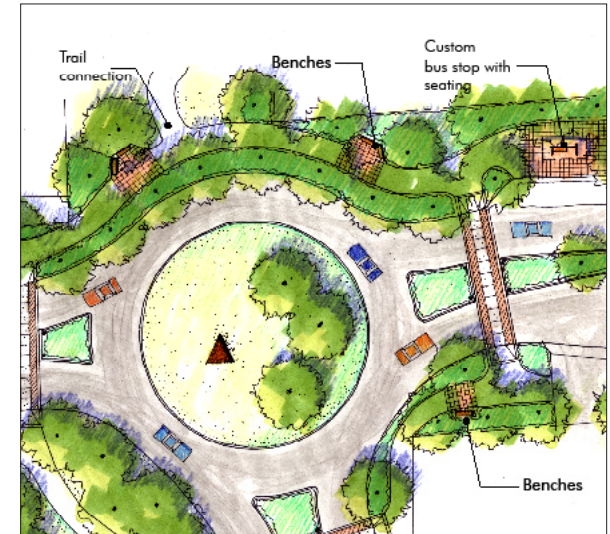
Provide multiple options for seating by varying size, materials, configurations, etc.

2viii-4

Assure that seating that is a fixed part of the building does not compromise the minimum five feet of unobstructed access required by the ADA.

2viii-5

Provide additional seating at regular intervals as the development intensifies along the West Capitol Avenue corridor.



Benches at the Sycamore Intersection



Benches at key locations



2 A I X S I G N A G E



Signage along median

2Aix-1

Ensure public signage follows a palette that reinforces the identity of West Capitol Avenue. Coordinate colors, shapes and graphics of signage with the City's signage system.

2Aix-2

Celebrate the sense of place with public signage on sidewalks and medians, and private signage on individual businesses along West Capitol Avenue. Emphasize gateways at Garden Street, Harbour Boulevard and Jefferson Boulevard.

2Aix-3

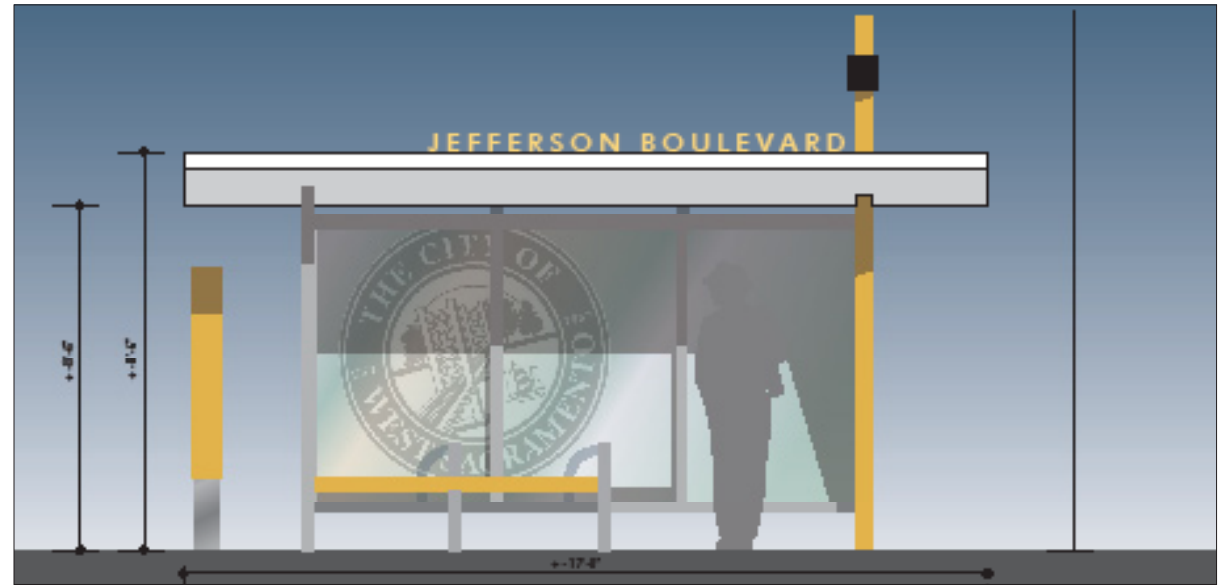
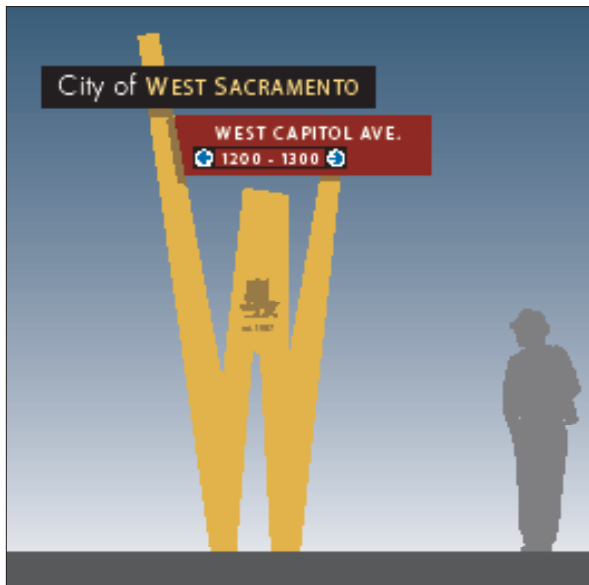
Incorporate public signage at bus stops.

2Aix-4

Use signage for wayfinding.

2Aix-5

Use signage to emphasize key locations, intersections, focal points and areas near civic buildings, such as City Hall.



Bus shelters with unique signage

2Ax BICYCLE PARKING

2Ax-1

Provide bicycle racks intermittently along the entire corridor, at least once every block, and more frequently if demand exists.

2Ax-2

Artistic design of public bicycle racks shall be implemented at key locations.

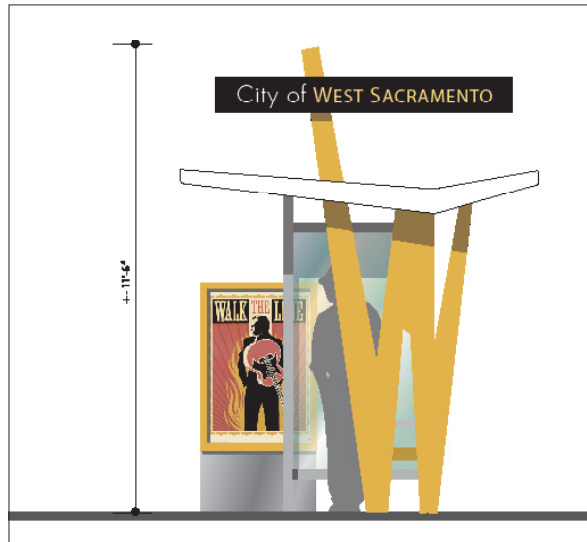
2Ax-2

Private development shall support the presence of public bicycle parking with additional bicycle parking opportunities onsite.

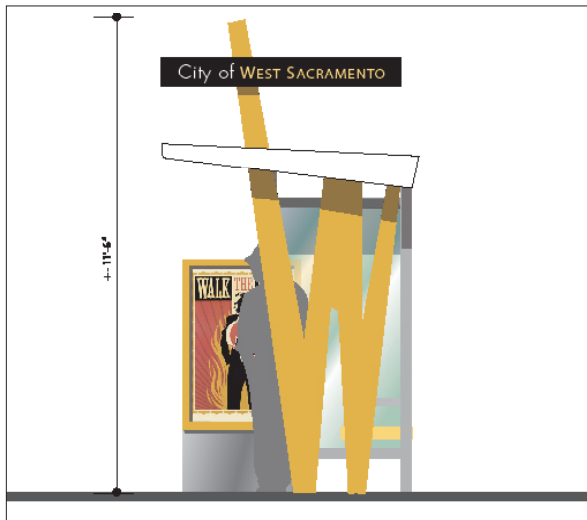


Artistic design of bicycle racks (2Ax-2)

2 AXI PUBLIC TRANSIT



Bus shelters for sidewalks wider than ten feet



Bus shelters for sidewalks ten feet wide

Buses

2Axi-1

Buses should travel in through lanes along multi-way sections of West Capitol Avenue.

2Axi-2

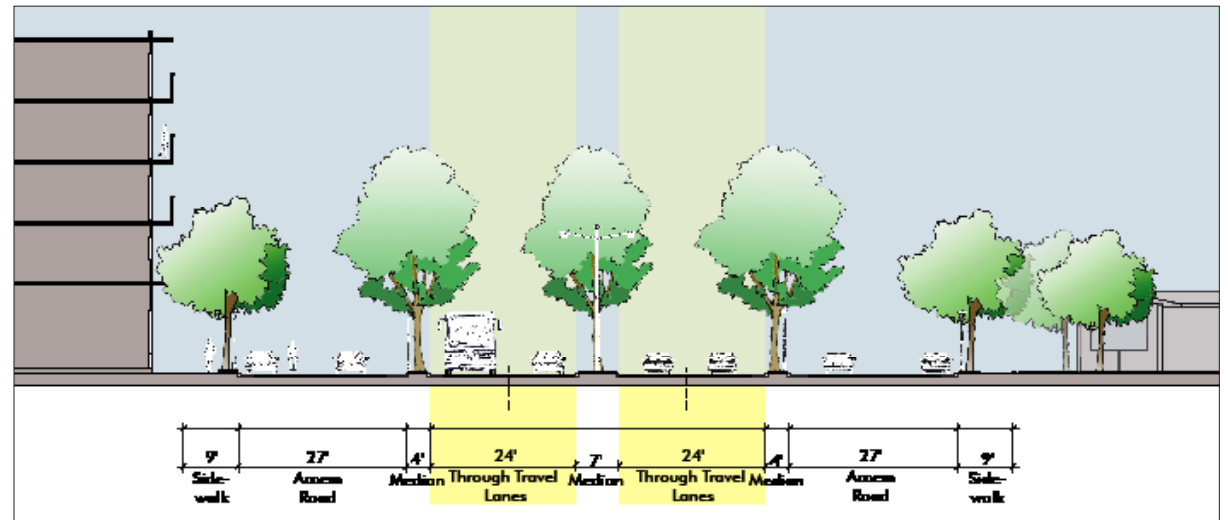
Provide bus stops every 1,200 feet along the corridor.

2Axi-3

Provide shelter, shade, seating and trash receptacles at each bus stop.

2Axi-4

Design bus stops to reinforce the identity of West Capitol Avenue and explore opportunities for individual identity at certain key bus stops / transfer points.



Buses in through traffic lanes in multi-way boulevard (2Axi-1)

2 AXI PUBLIC TRANSIT

Trolley/Streetcar/Light Rail

2Axi-5

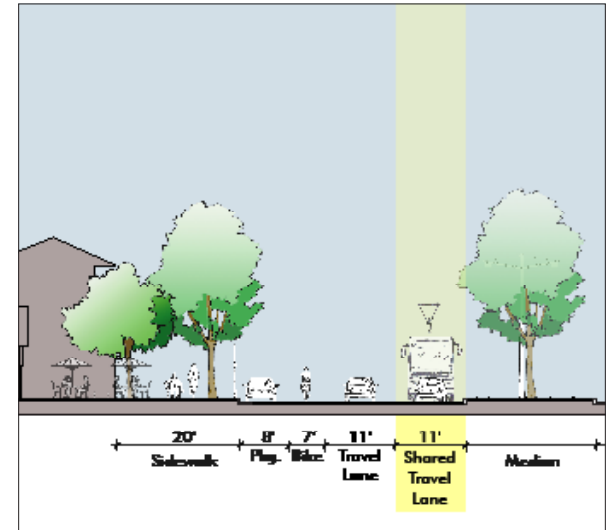
Future rail should either share or replace an existing travel lane.

2Axi-6

Trolley and light rail stops and boarding platforms should be placed in the median in the Downtown Core and Gateway districts.

2Axi-7

In the multi-way boulevard sectors, explore the placement and design of stops, such as the widening of sidewalks or the widening of the side medians to meet the rail tracks.



Future rail (2Axi-5)



